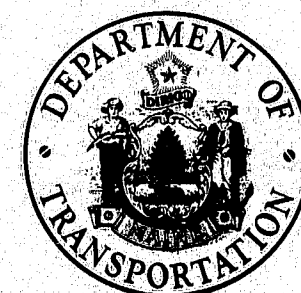


STATE OF MAINE DEPARTMENT OF TRANSPORTATION



SPECIFICATIONS

DESIGN----- A.A.S.H.T.O. Standard Specifications for Highway Bridges 1979.
CONTRACT----- State of Maine, State Highway Commission, Standard Specifications,
Highways and Bridges, Revision of June 1968.

MATERIALS

CONCRETE----- class "A"
REINFORCING STEEL----- A.S.T.M. A615 grade 60

BASIC ALLOWABLE STRESSES

CONCRETE----- $f_c = 1200$ psi, $n=9$
REINFORCING STEEL----- $f_s = 24,000$ psi

BUREAU OF HIGHWAYS BRIDGE RAIL I-95 OVER BOND BROOK & MT. VERNON AVE.

IN THE CITY OF AUGUSTA

LENGTH OF PROJECT 0.087 MILE

I-95 OVER M.C.R.R. & COUNTY ROAD

IN THE CITY OF WATERVILLE

LENGTH OF PROJECT 0.059 MILE

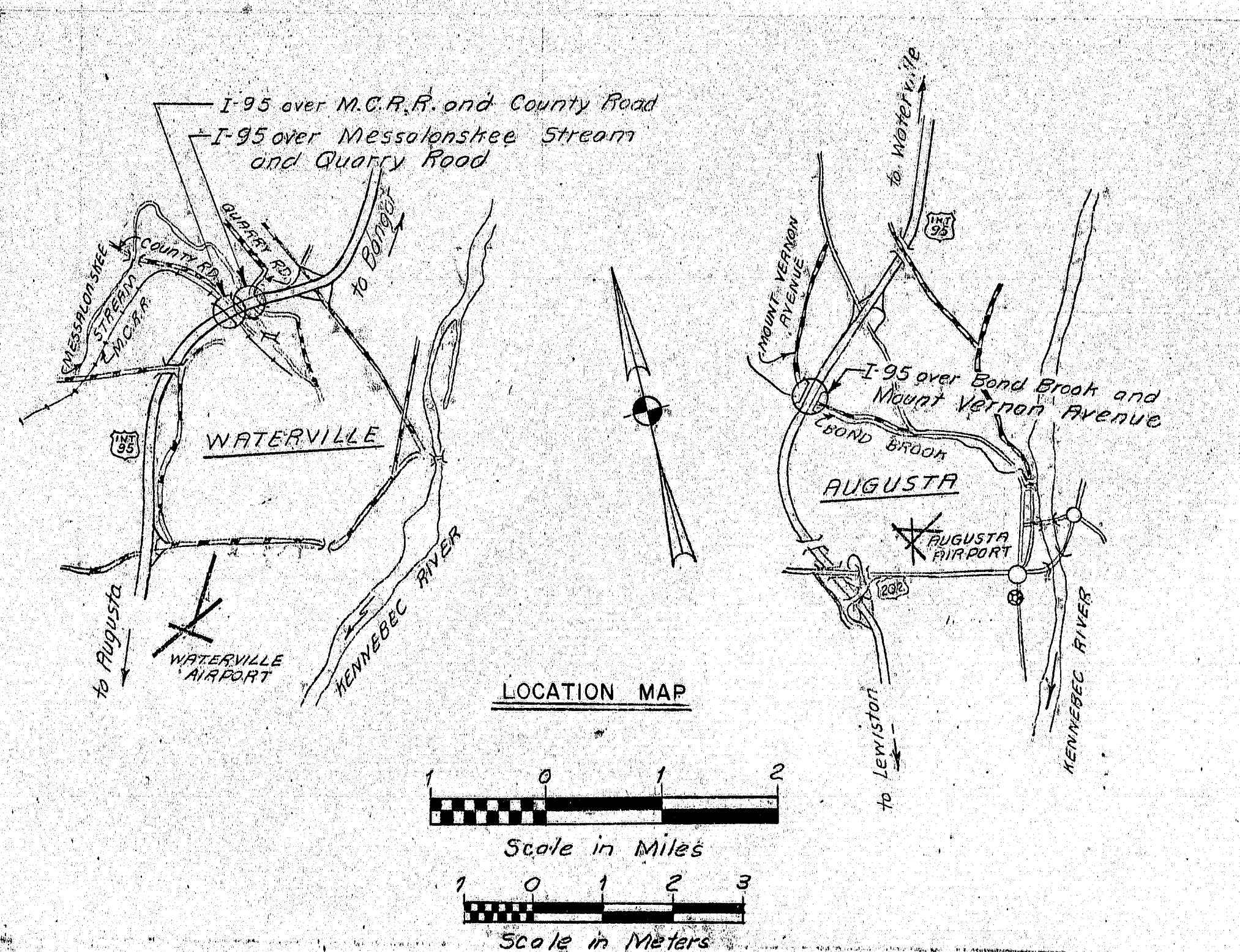
I-95 OVER MESSALONSKEE STREAM & QUARRY ROAD

IN THE CITY OF WATERVILLE

LENGTH OF PROJECT 0.209 MILE

PROJECT NO. I-IG-95-6 (54) 105

REVISED AS BUILT 1-26-81

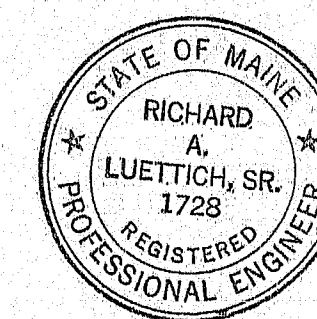


I-95 OVER BOND BROOK & MT. VERNON AVE.

TRAFFIC DATA	ONE DIR.	BOTH DIR.
A.D.T. 1980	5335	10670
A.D.T. 2000	7700	15400
D.H.V.	1186	2156
T (%)	14	14
D. (%)	100	55
V.		
P.S.D. (%)		
18 KIPS	829	

I-95 OVER M.C.R.R. & COUNTY ROAD I-95 OVER MESSALONSKEE STREAM & QUARRY ROAD

TRAFFIC DATA	ONE DIR.	BOTH DIR.
A.D.T. 1980	6060	12120
A.D.T. 2000	8750	17500
D.H.V.	1251	2275
T (%)	14	14
D. (%)	100	55
V.		
P.S.D. (%)		
18 KIPS	932	



APPROVED:

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
Richard A. Luettich, Sr.
REGISTERED PROFESSIONAL ENGINEER
BUREAU DIRECTOR AND CHIEF ENGINEER

DATE

4-22-80

4-22-80

As Built 1-26-81

UNITED STATES
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 1

APPROVED:

DIVISION ENGINEER DATE

172-115

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	1	18

INDEX OF SHEETS

TITLE SHEET	1
ESTIMATED QUANTITIES	2
TEMPORARY BARRIER	3
I-95 OVER BOND BROOK & MOUNT VERNON AVENUE	4
GENERAL PLAN	4
CURBS & RAILINGS	5
I-95 OVER M.C.R.R. & COUNTY ROAD	6
GENERAL PLAN	6
CURBS & RAILINGS	7
I-95 OVER MESSALONSKEE STREAM & QUARRY ROAD	8
GENERAL PLAN	8
CURBS & RAILINGS	9
CURB SECTIONS	10
REINFORCING STEEL SCHEDULE	11
ALUMINUM BRIDGE RAILING	12
CONCRETE END POSTS	13
GUARD RAILS	14
FIELD OFFICE	15
MAINTENANCE OF TRAFFIC	16-18

Plans of the existing bridge are available for the Contractor's reference at the Bridge Design Office in Augusta. The plans are reproductions of original drawings as prepared for the construction of the bridge and it is very unlikely that the plans will show any construction field changes or any alterations which may have been made to the bridge during its life span.

PROJECT DESIGN ENGINEER	DATE
DESIGNER	2/80
CHECKED	
REVISIONS	
FIELD CHANGES	
PLANS	

ESTIMATED QUANTITIES							
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	QUANTITY	QUANTITY	QUANTITY	QUANTITY
202.12	Removal of Existing Structural Concrete	C.Y.	186	134	525	845	
202.13	Removal of Existing Railings (Retained by Department)	L.F.	1634	1004	4162	6800	
202.20	Removing Bituminous Concrete Pavement	S.Y.	6	None	6	12	
202.201	Removing Bituminous Pavement (Waterproof Membrane)	S.Y.	90	60	227	377	
206.08	Str. Earth Excav. - Abuts. and Ret. Walls	C.Y.	None	None	11	11	
403.101	Hot Bit. Pavement, Grading D. (Sidewalks, Drives, Etc.)	Ton	37	26	105	168	
502.42	Struct. Conc. Roadway & Sidewalk Slabs on Steel Bridges	C.Y.	78	58	225	361	
503.12	Reinforcing Steel, Fabricated and Delivered	Lb.	11,614	7,655	27,331	46,600	
503.13	Reinforcing Steel, Placing	Lb.	11,614	7,655	27,331	46,600	
507.141	Aluminum Bridge Railing, Type "A"	L.F.	1,716	1,007	4,147	6,870	
508.10	Membrane Waterproofing	S.Y.	212	137	531	880	
514.06	Curing Bar for Concrete Cylinders	Each	0.25	0.15	0.60	1.0	
515.20	Protective Coating for Concrete Surfaces	S.Y.	462	306	1157	1925	
526.30	Temporary Concrete Barrier Type 1	L.F.	780	600	1400	2780	
526.40	Resetting Temporary Concrete Barrier Type 1	L.F.	1380	1000	1600	3,980	
606.364	Guard Rail, Remove, Modify, Reset, Type 3b	L.F.	100	100	100	300	
606.367	Replace Unusable Existing Guard Rail Posts	Each	2	2	2	6	
609.13	Vertical Bridge Curb - Type 1	L.F.	1632	1041	4144	6817	
609.38	Resetting Curb, Type-1	L.F.	None	200	200	400	
629.05	Labor, Straight Time	M.Hr.	25	15	60	100	
630.067	Traffic Officers	M.Hr.	20	20	40	80	
631.10	Air Compressor (inc. operator)	Hour	25	15	60	100	
631.11	Air Tool (inc. operator)	Hour	25	15	60	100	
631.171	Truck - Small (inc. operator)	Hour	25	15	60	100	
631.22	Front End Loader (inc. operator)	Hour	5	3	12	20	
632.08	Warning Lights	Grp.	2	1	1	4	
639.09	Field Office, Type B	Each	0.25	0.15	0.60	1.0	
645.61	4" Solid White Pavement Marking Line	L.F.	950	570	2,280	3,800	
645.63	4" Solid Yellow Pavement Marking Line	L.F.	950	570	2,280	3,800	
645.71	Temporary 4" Plastic Pavement Marking Line	L.F.	1,980	1,190	4,750	7,920	
652.30	Flashing Arrow Board	Each	1.0	0.5	0.5	2	
652.31	Type 1 Barricades	Each	25	12	13	50	
652.33	Drums	Each	5	2	3	10	
652.34	Cones	Each	13	6	6	25	
652.35	Construction Signs	S.F.	225	112	113	450	
652.36	Maintenance of Traffic Control Devices	C.D.	43	23	102	170	
659.10	Mobilization	L.S.	0.25	0.15	0.60	1.0	

F.H.W.A. RES. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)05	2	18

GENERAL NOTES

1) The structural steel in the curb portions of the deck joints shall be removed. The removal, including cutting and grinding at the weld to the roadway dam shall be considered incidental to Item 202.12 Removal of Existing Structural Concrete.

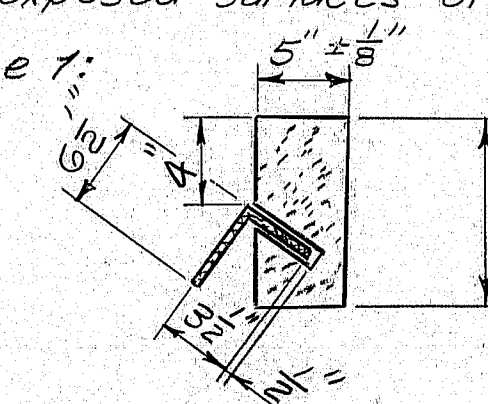
2) All concrete will be paid for under Item 502.42 Structural Concrete, Roadway and Sidewalk Slabs on Steel Bridges.

3) Chamfer all exposed edges of concrete a consistent dimension between $\frac{1}{8}$ " and $\frac{3}{8}$ " inclusive, unless otherwise indicated.

4) Reinforcing steel shall have 2 inches cover unless otherwise indicated.

5) Protective coating shall be applied to all exposed surfaces of new concrete.

6) Details for Vertical Bridge Curb - Type 1:



7) The granite curbing along the return wings at the westerly abutments of the Messalonskee Stream bridge shall be retained by the Department. The curbing shall be carefully removed by the Contractor and loaded onto trucks supplied by the Department, or stored within the right-of-way. Payment for the removal and salvage of the curbing shall be made under Item 206.08. Structural Earth Excavation - Abutments and Retaining Walls.

8) Holes for drilling and grouting shall be $1\frac{1}{2}$ " to 2" in diameter, inclusive. Holes shall be filled with water for a minimum of two hours immediately prior to grouting, at which time all excess water shall be removed. The grouted area around the reinforcing bar shall be kept wet, from the time of initial set, for a minimum of 12 hours with burlap or other suitable means. The grout shall be used in accordance with the manufacturer's recommendations and be on the Department's List of Approved Non-shrink Grouts. No separate payment will be made for drilling and grouting and all such work will be considered incidental to Item 503.13, Reinforcing Steel, placing.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
OVER
BOND BROOK
COUNTY ROAD
MESSALONSKEE STREAM
IN THE CITIES OF
AUGUSTA & WATERVILLE
ESTIMATED QUANTITIES

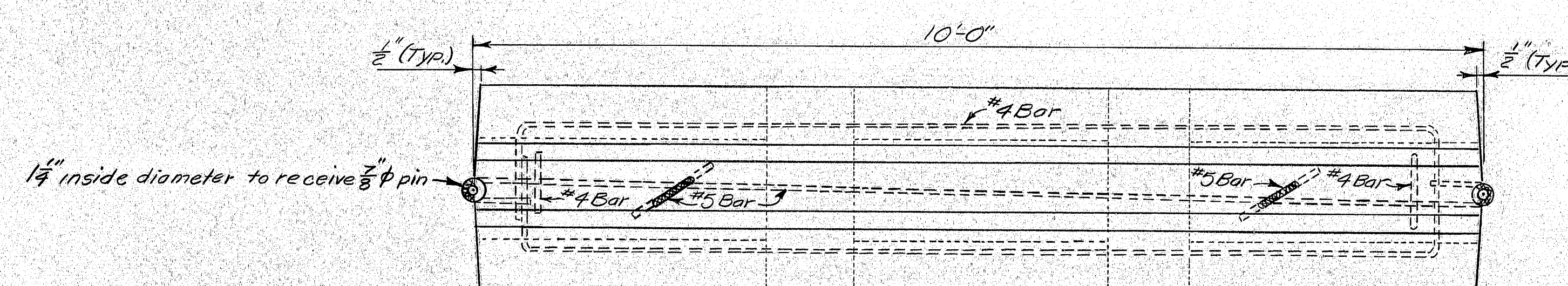
SHEET 2 OF 18 AUGUSTA, MAINE Nov. 1979

172-116

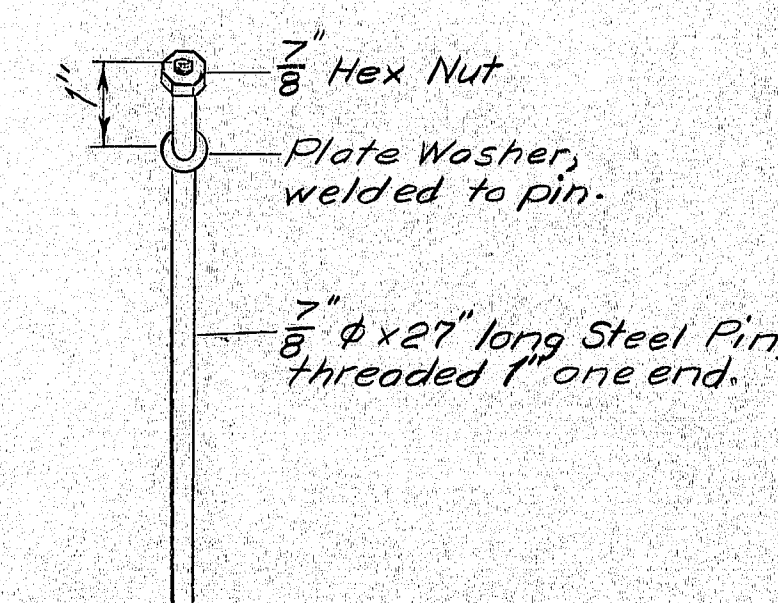
P.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	3	18

CONCRETE BARRIER NOTES:

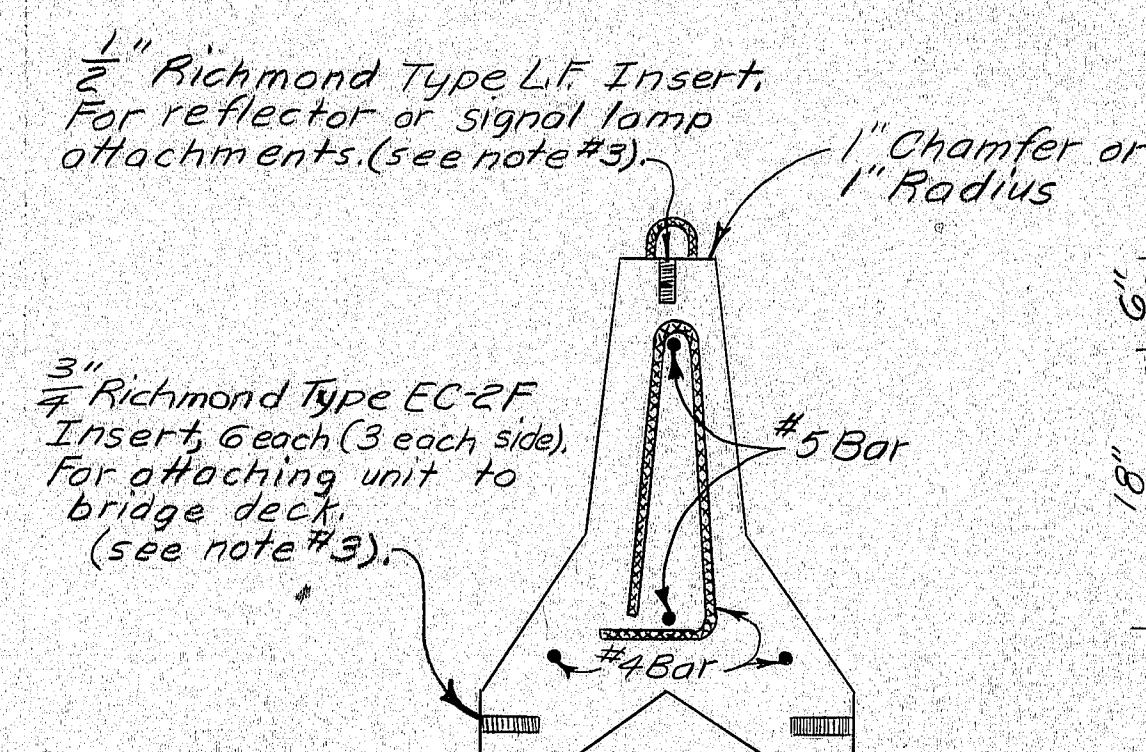
1. ~ Other reinforcing details and panel connection arrangements may be used, if approved by the Engineer, for Temporary Concrete Barrier Type 1.
2. ~ Install Yellow ReflectORIZED Plastic Pavement Marking Material on temporary concrete barrier panels at 20'-0" max. spacings. Material to be considered incidental to Item 526.30.
3. ~ The Richmond Inserts will not be required under this contract but recommended if the temporary barrier is to be used in the future.



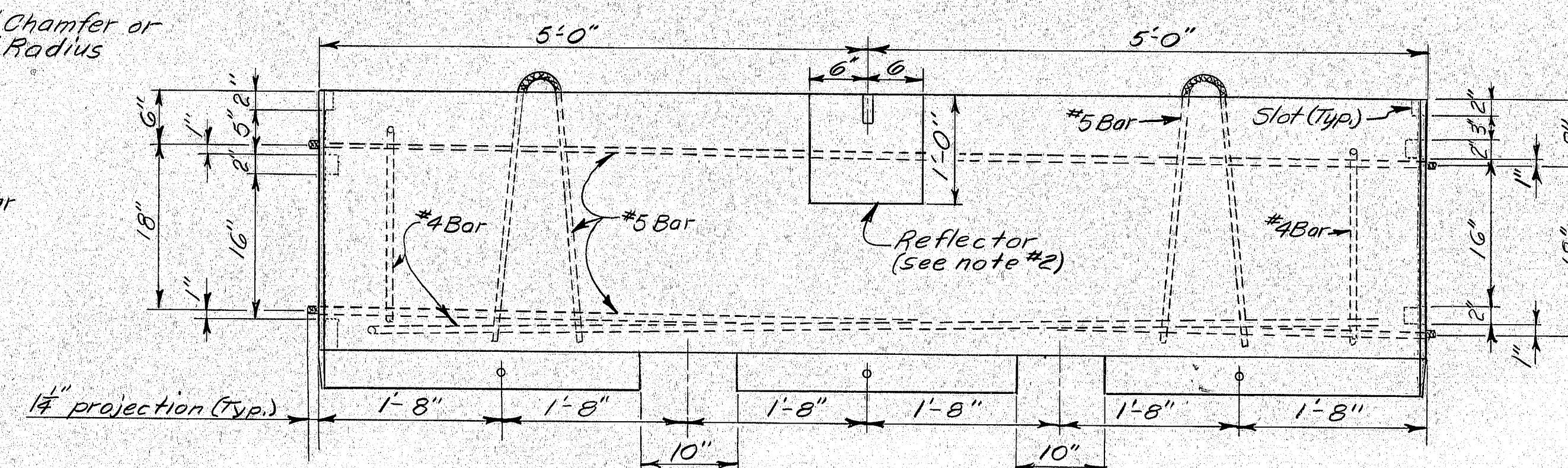
PLAN



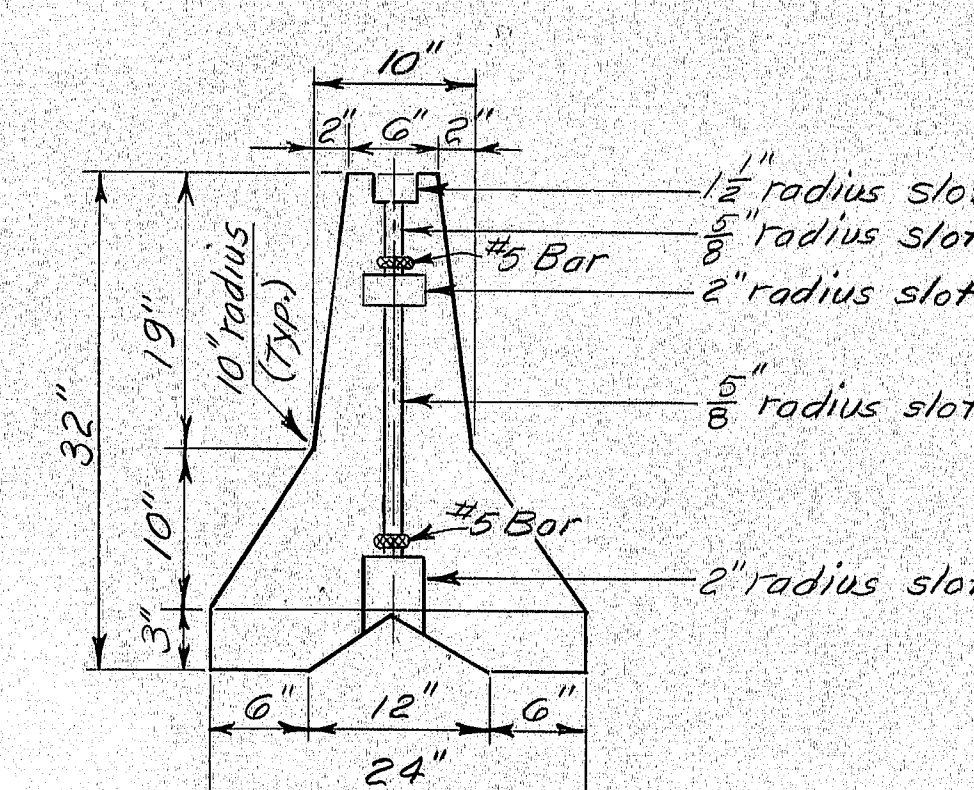
CONNECTING PIN DETAIL



TYPICAL SECTION



ELEVATION



TYPICAL END VIEW

TEMPORARY CONCRETE BARRIER PANEL
TYPE-I

PROJECT DESIGN ENGINEER	DATE
BY	1-80
DESIGN - DETAILED	1-80
CHECKED	2-80
REVISIONS	
FIELD CHANGES	

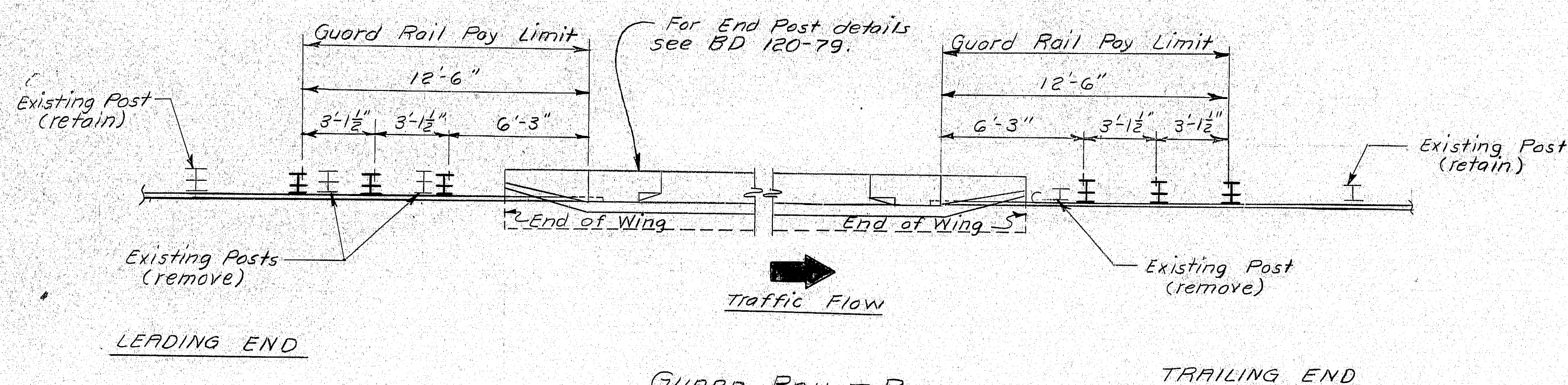
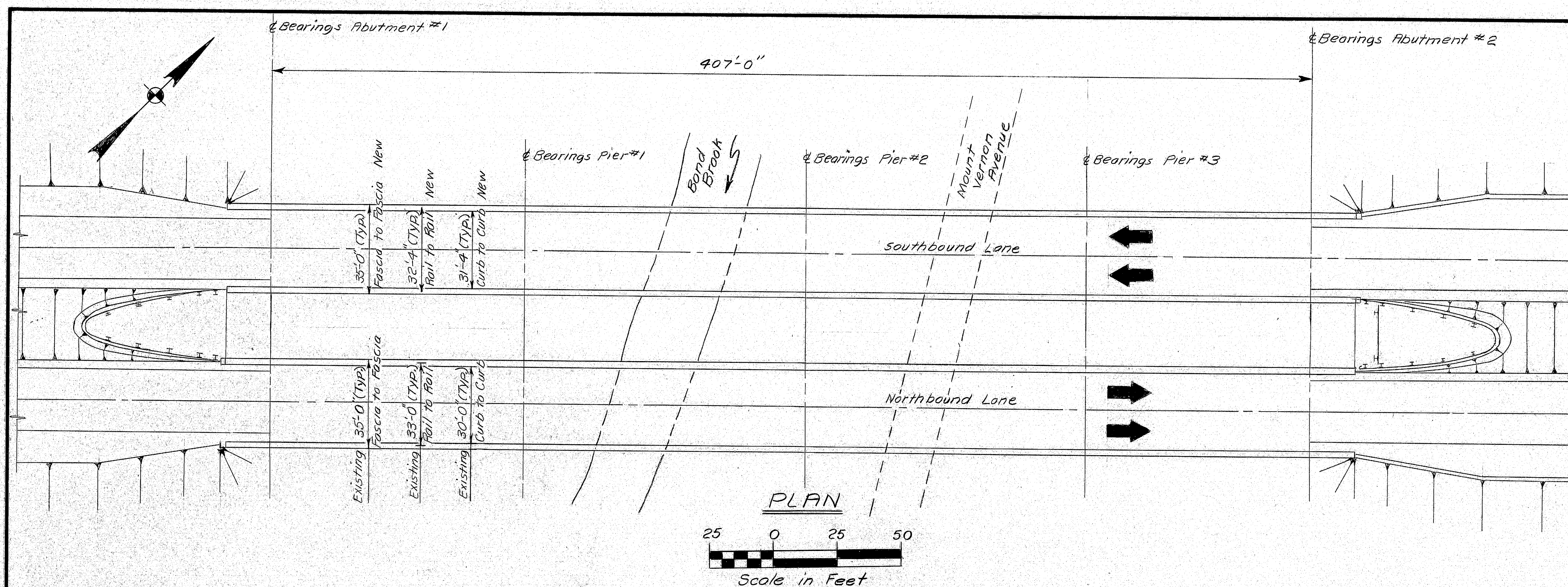
STATE OF MAINE DEPARTMENT OF TRANSPORTATION
INTERSTATE 95 OVER BOND BROOK COUNTY ROAD MESSALONSKEE STREAM IN THE CITIES OF AUGUSTA & WATerville TEMPORARY BARRIER
SHEET 3 OF 18 AUGUSTA, MAINE Jan. 1980

172-117

F.R.W.B. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	4	18

NOTES

1. Northbound travel lane milled down 1 1/4" between expansion joints and repaved with "C" mix.
2. Bituminous hand sealing applied to new hot-top curb area 4' wide NB right, 3' wide other three curbs.



GUARD RAIL - PLAN
One of four shown, others similar

PROJECT DESIGN ENGINEER	DATE
JET	10-79
DESIGN - CHECKED	JET
REVISIONS	G.O.T.
FIELD CHANGES	2/80

Revised as Built D. Sherlock 1-26-81

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

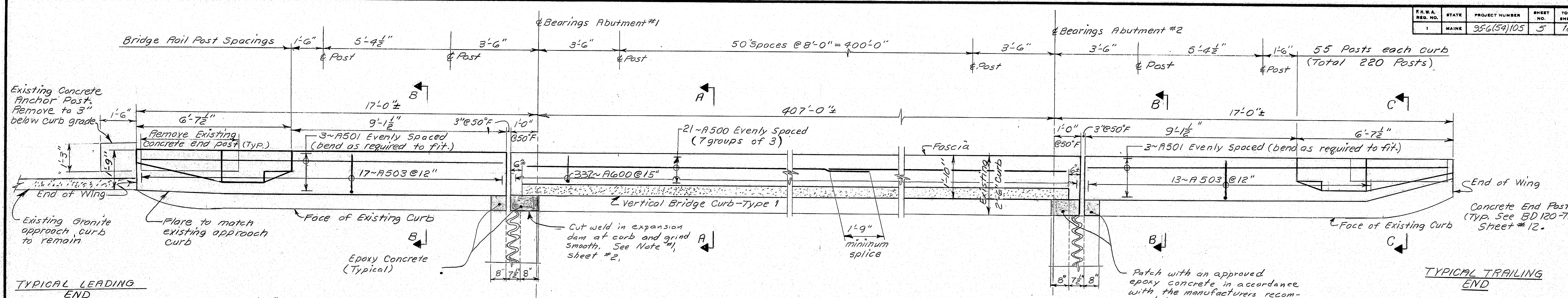
INTERSTATE 95
OVER
BOND BROOK &
MOUNT VERNON AVENUE
IN THE CITY OF
AUGUSTA
KENNEBEC COUNTY

GENERAL PLAN

SHEET 4 OF 18 AUGUSTA, MAINE Nov. 1979

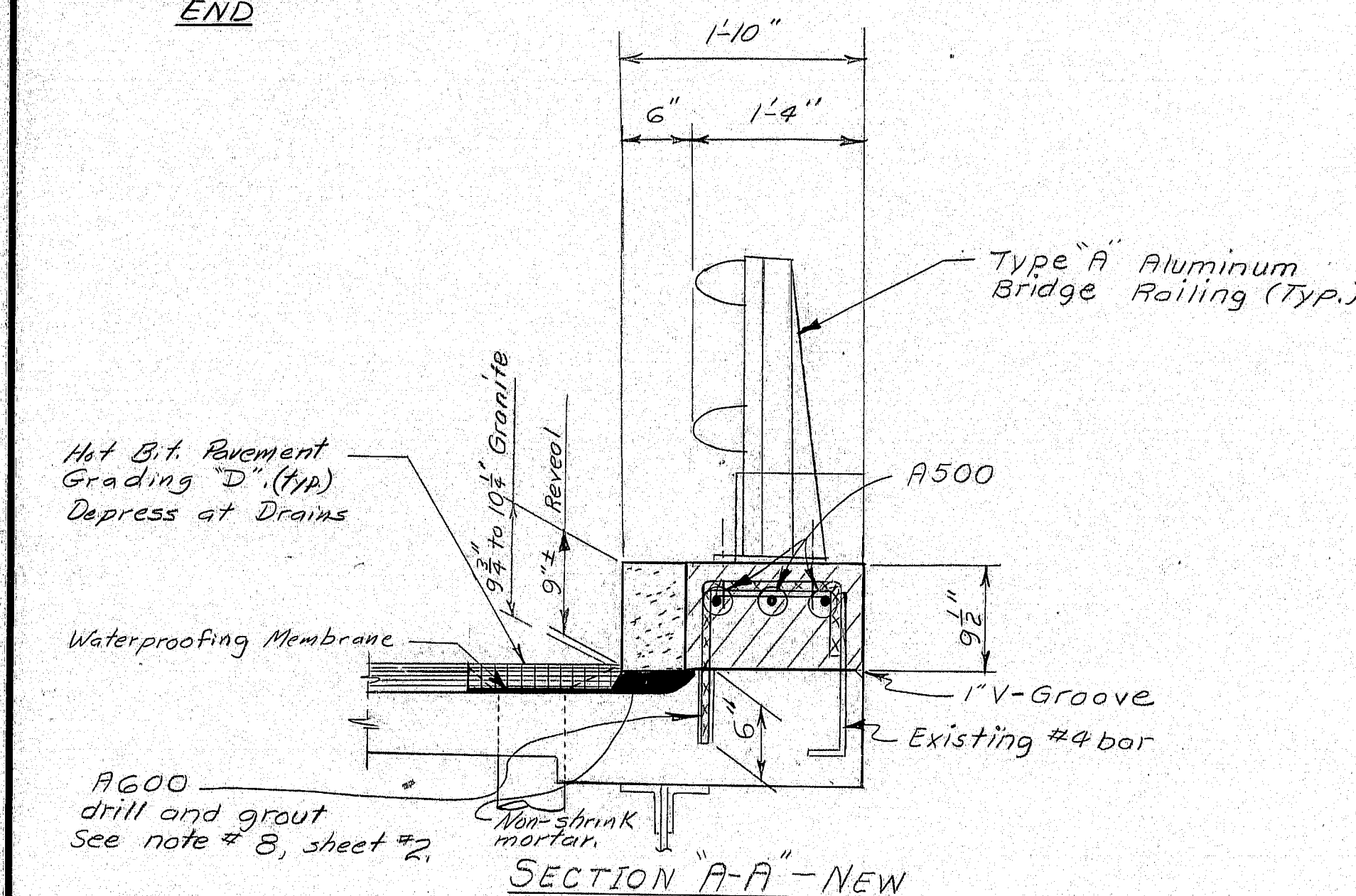
172-118

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TO SHEET
1	MAINE	95-6(54)105	5	10

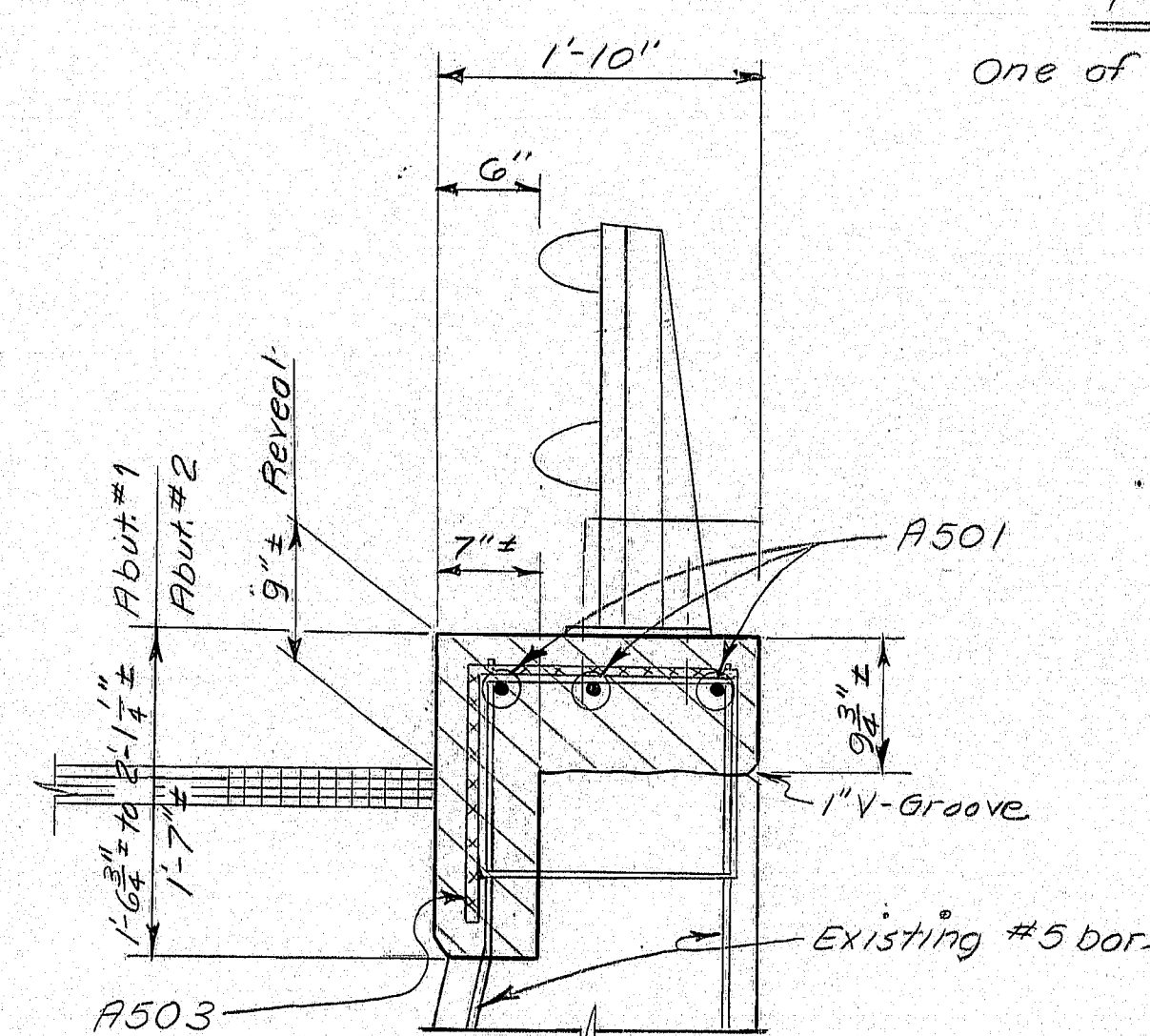


PLAN - CURE

One of four shown, others similar

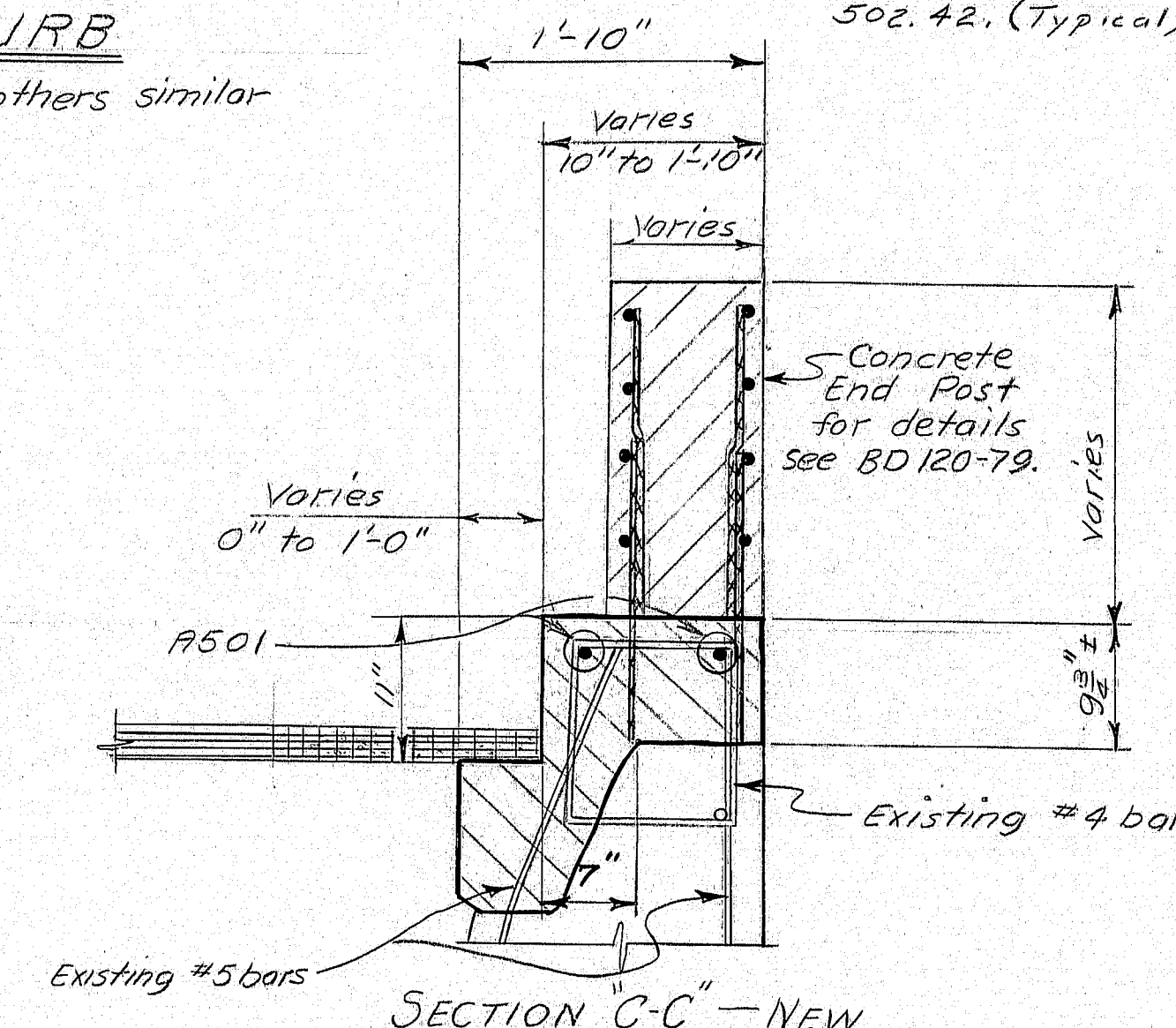


SECTION "A-A" - NEW

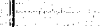






SECTION "B-B" - NEW

NOTE: Curb grade shall be $\frac{3}{4}$ " higher than existing curb grade.



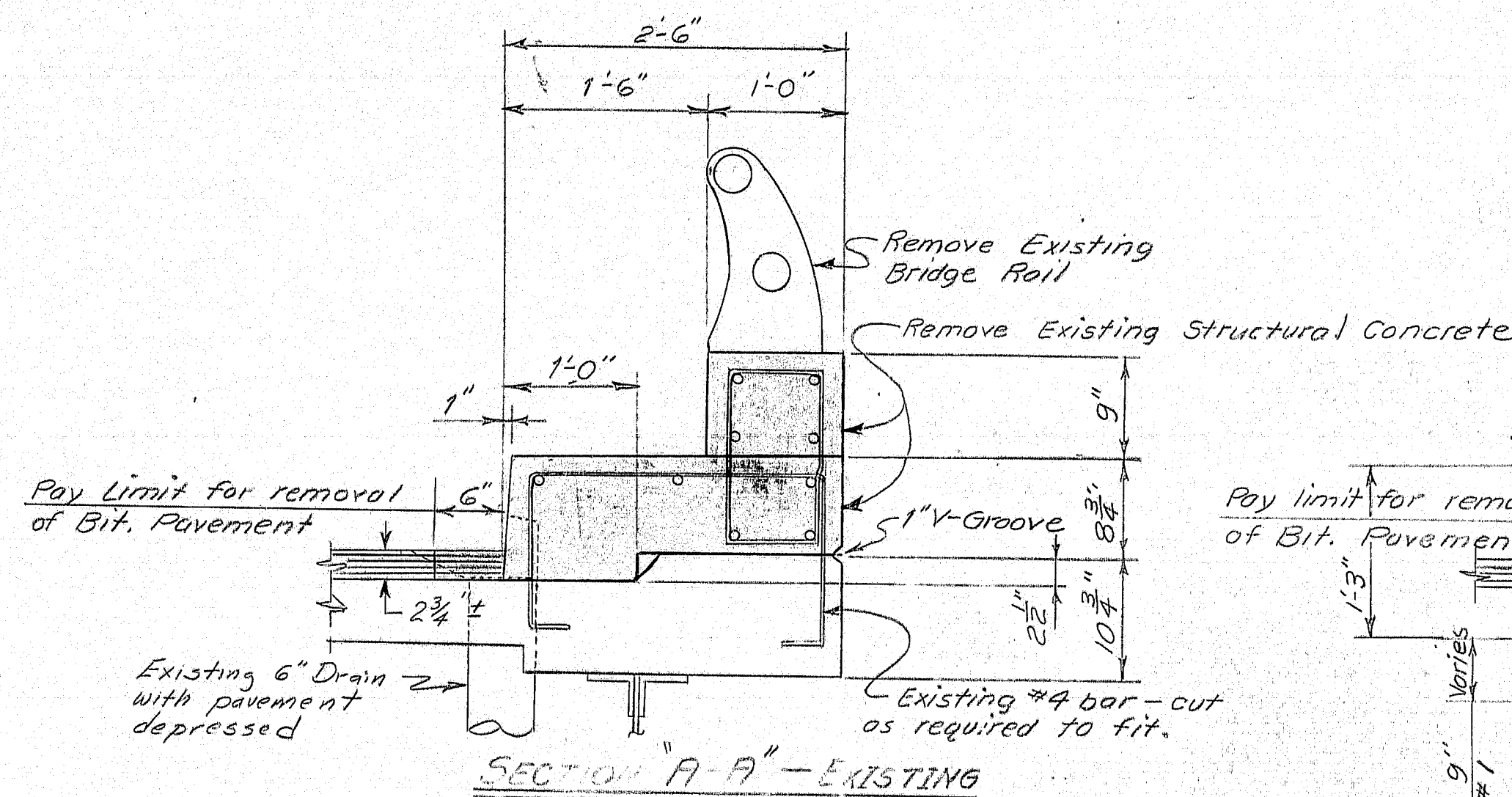
SECTION "C-C"—NEW

- | | |
|---|----------------------------------|
|  | Existing Concrete |
|  | Existing material to be removed |
|  | New Concrete |
|  | Existing Hot Bituminous Pavement |
|  | New Hot Bituminous Pavement |

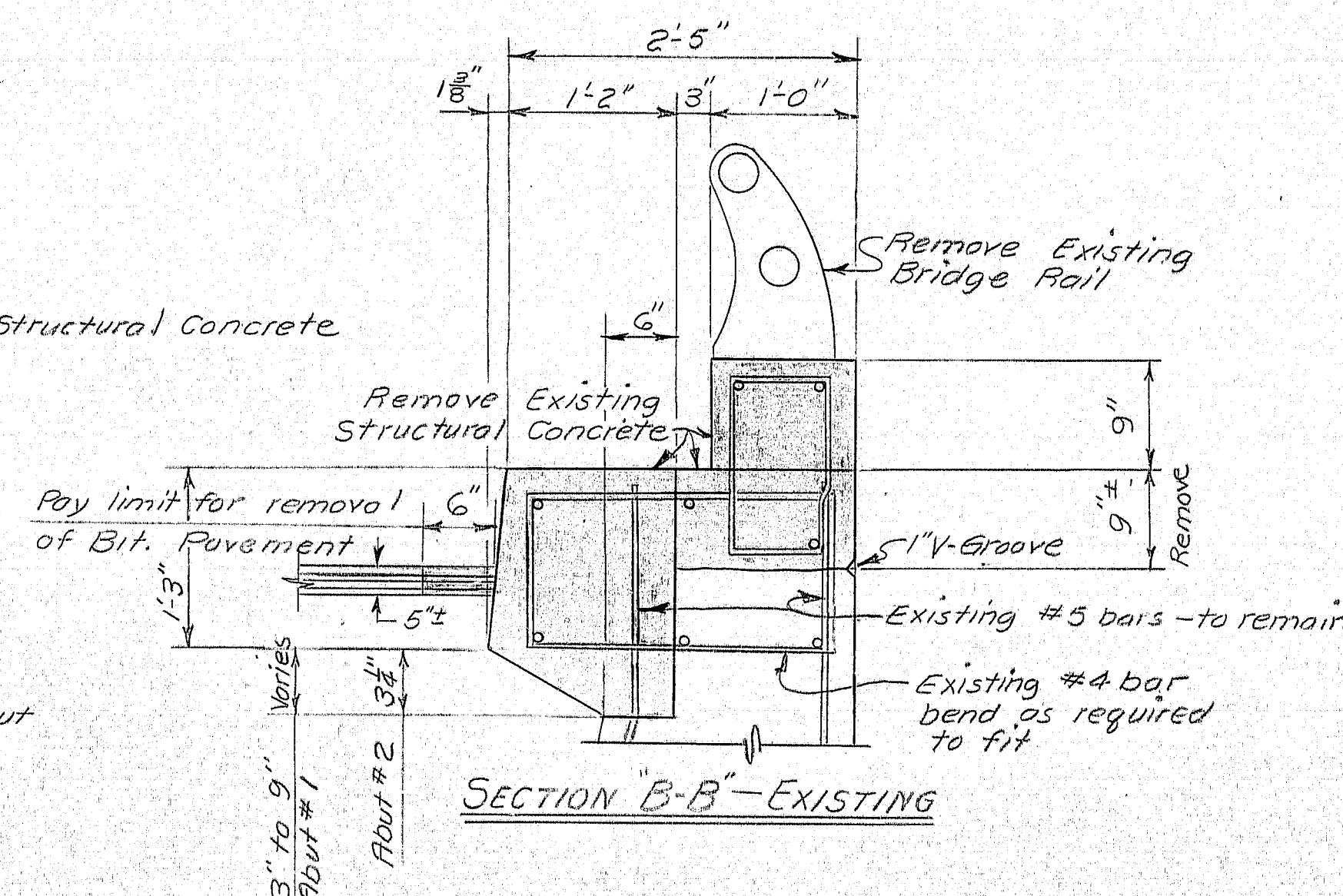
LEGEND OF SECTIONS

REFERENCES

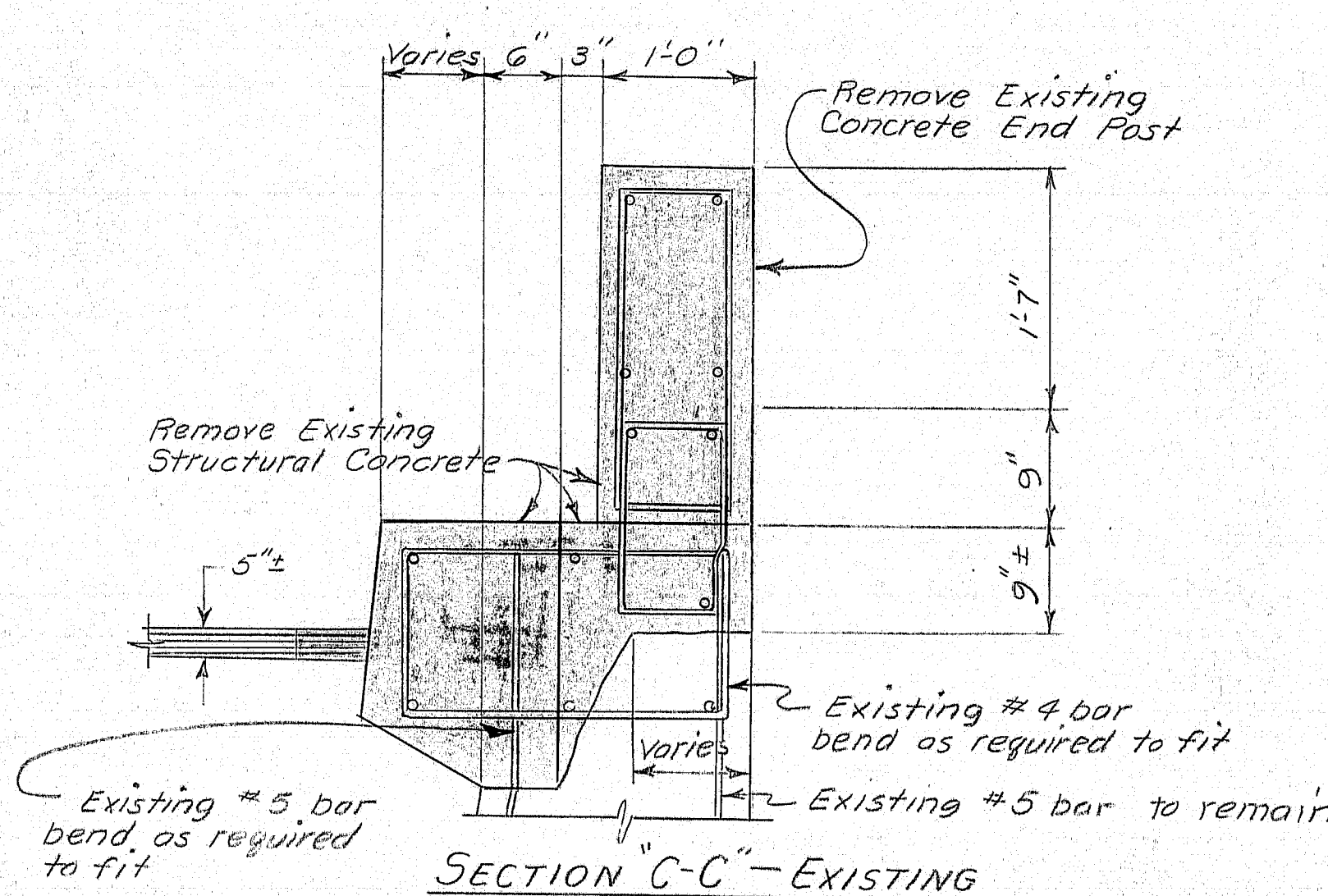
Concrete End Posts -- BD 120-79 -- Sh.# 13
Aluminium Bridge Railing -- BD 114-77 -- Sh.# 12



SECTION "A-A" - EXISTING



SECTION "B-B"—EXISTING



SECTION "C-C" - EXISTING

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	JET	10-79
CHECKED	G.O.T.	2/82
REVIEWS		
FIELD CHANGES		

Revised as Built - D. Sherlock. 1-26-8

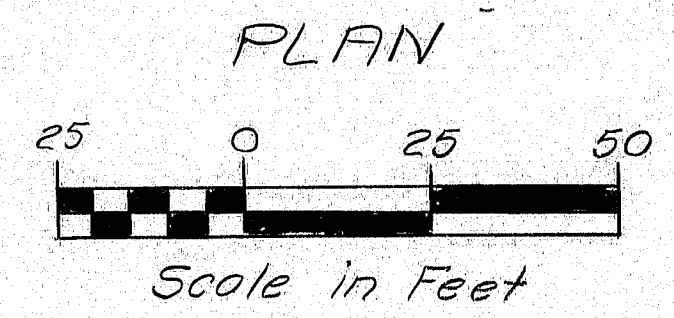
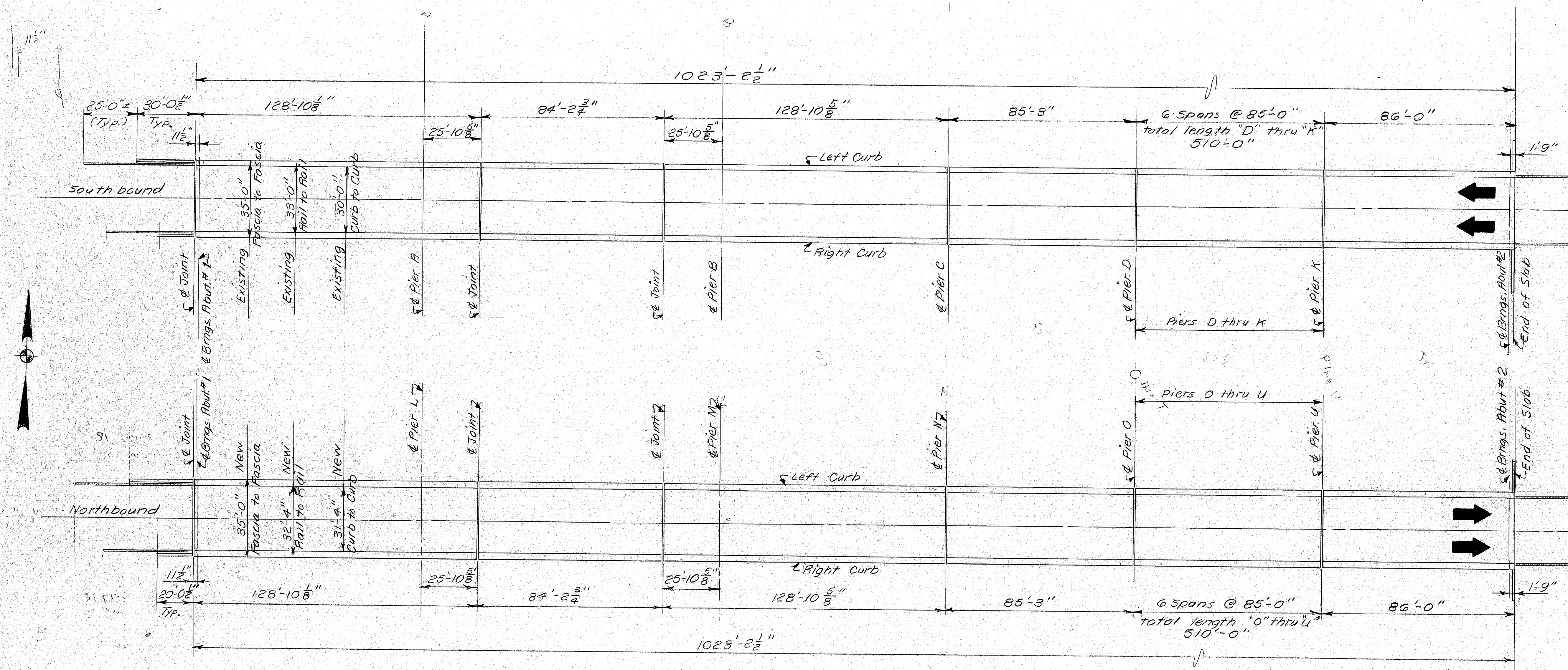
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
OVER
BOND BROOK &
MOUNT VERNON AVENUE
IN THE CITY OF
AUGUSTA
KENNEBEC COUNTY
CURBS & RAILINGS

SHEET 5 OF 18 AUGUSTA, MAINE Nov. 1979

172-119¹³⁻¹

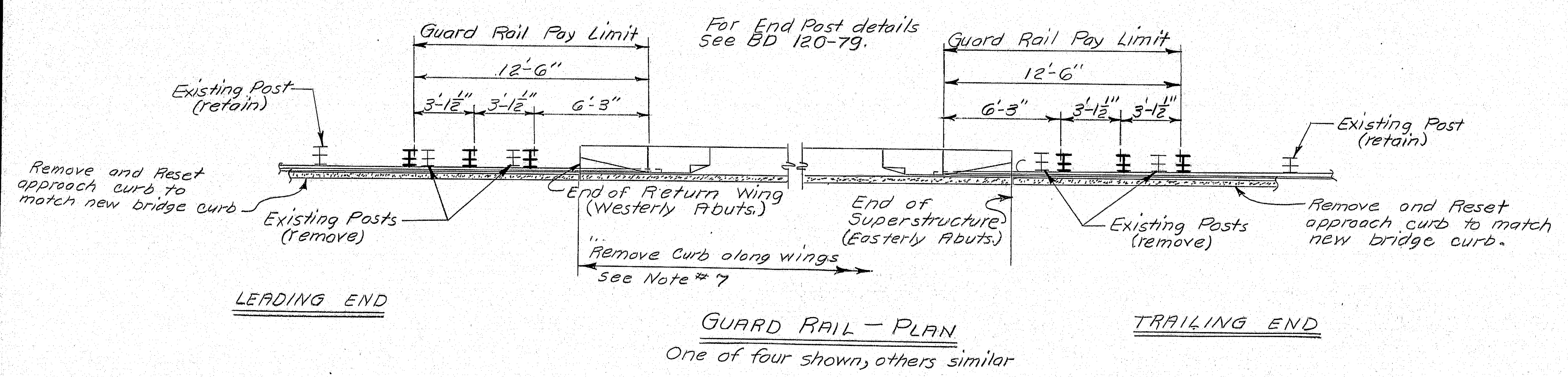
F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	8	18



NOTE

Bituminous hand sealing applied to new hot-top at curb area 3' wide.

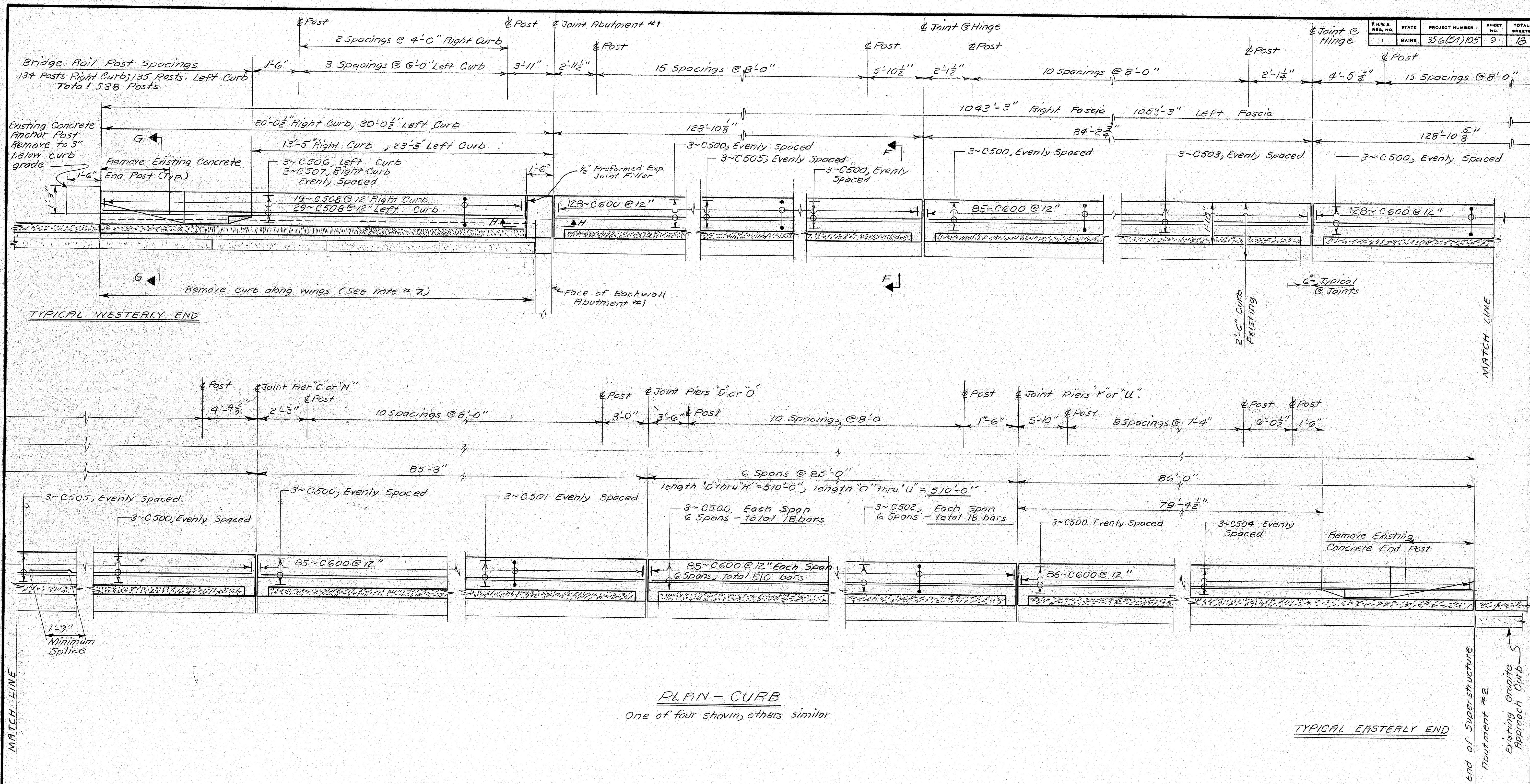
PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAIL	10/79
CHECKED	11/80
REVISIONS	
FIELD CHANGES	



Revised as Built - D. Sherlock, 1-26-81
STATE OF MAINE DEPARTMENT OF TRANSPORTATION
INTERSTATE 95 OVER MESSALONSKEE STREAM & QUARRY ROAD IN THE CITY OF WATERVILLE KENNEBEC COUNTY GENERAL PLAN
SHEET 8 OF 18 AUGUSTA, MAINE Nov. 1979

172-122

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	9	18



PLAN - CURB
One of four shown, others similar

PROJECT DESIGN ENGINEER	DATE
BY JET 10/29	
CHECKED G.O.T. 11/20	
REVISIONS	
FIELD CHANGES	

REFERENCES
Concrete End Posts BD120-79 Sh. #13.
Aluminum Bridge Railing BD114-77 Sh. #12.
Curb Sections Sh. #10.

Revised as Built-D. Sherlock, 1-26-81

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
OVER
MESSALONSKEE STREAM &
QUARRY ROAD
IN THE CITY OF
WATERVILLE
KENNEBEC COUNTY
CURBS & RAILINGS

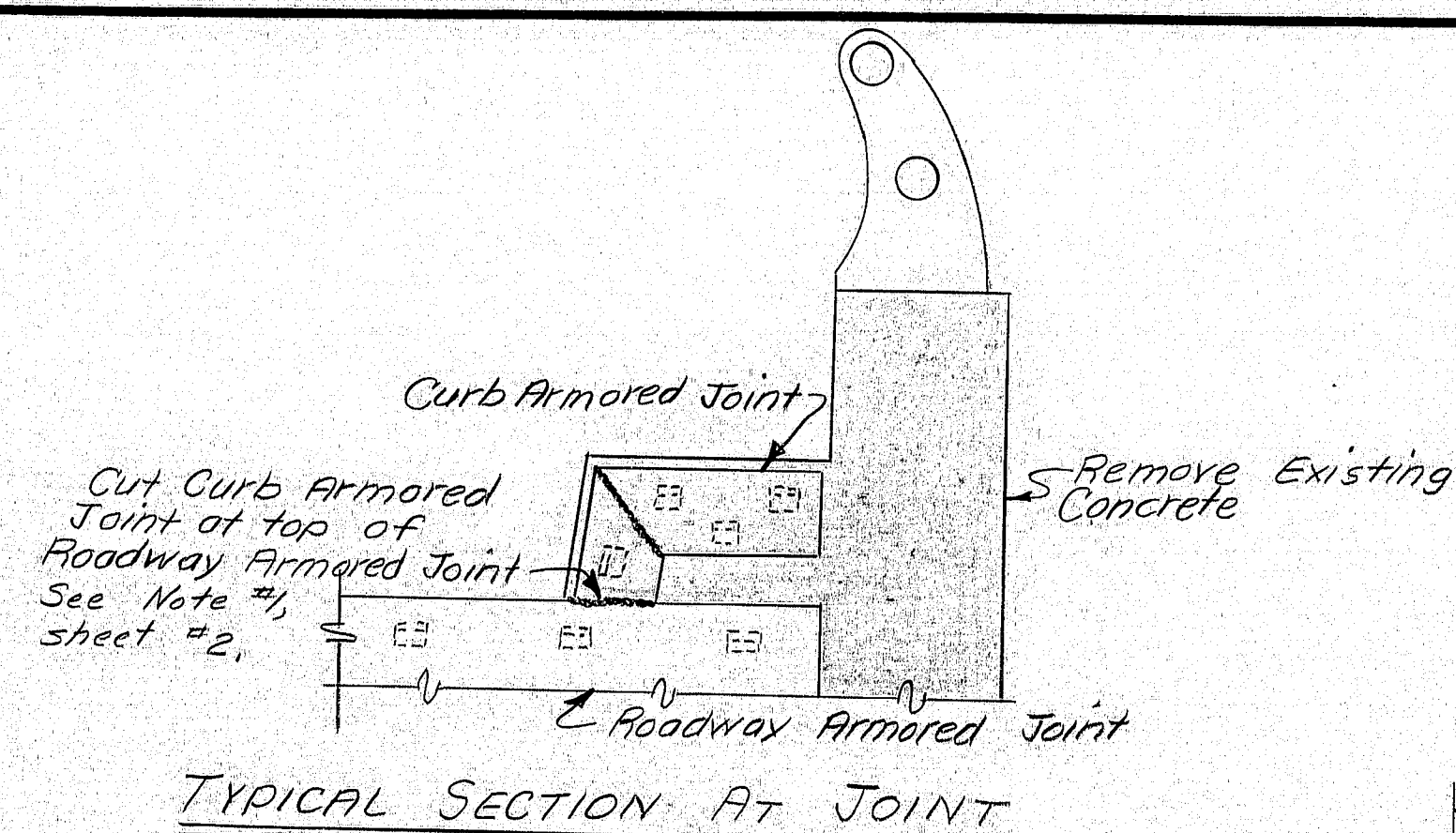
SHEET 9 OF 18 AUGUSTA, MAINE Nov. 1979

172-123

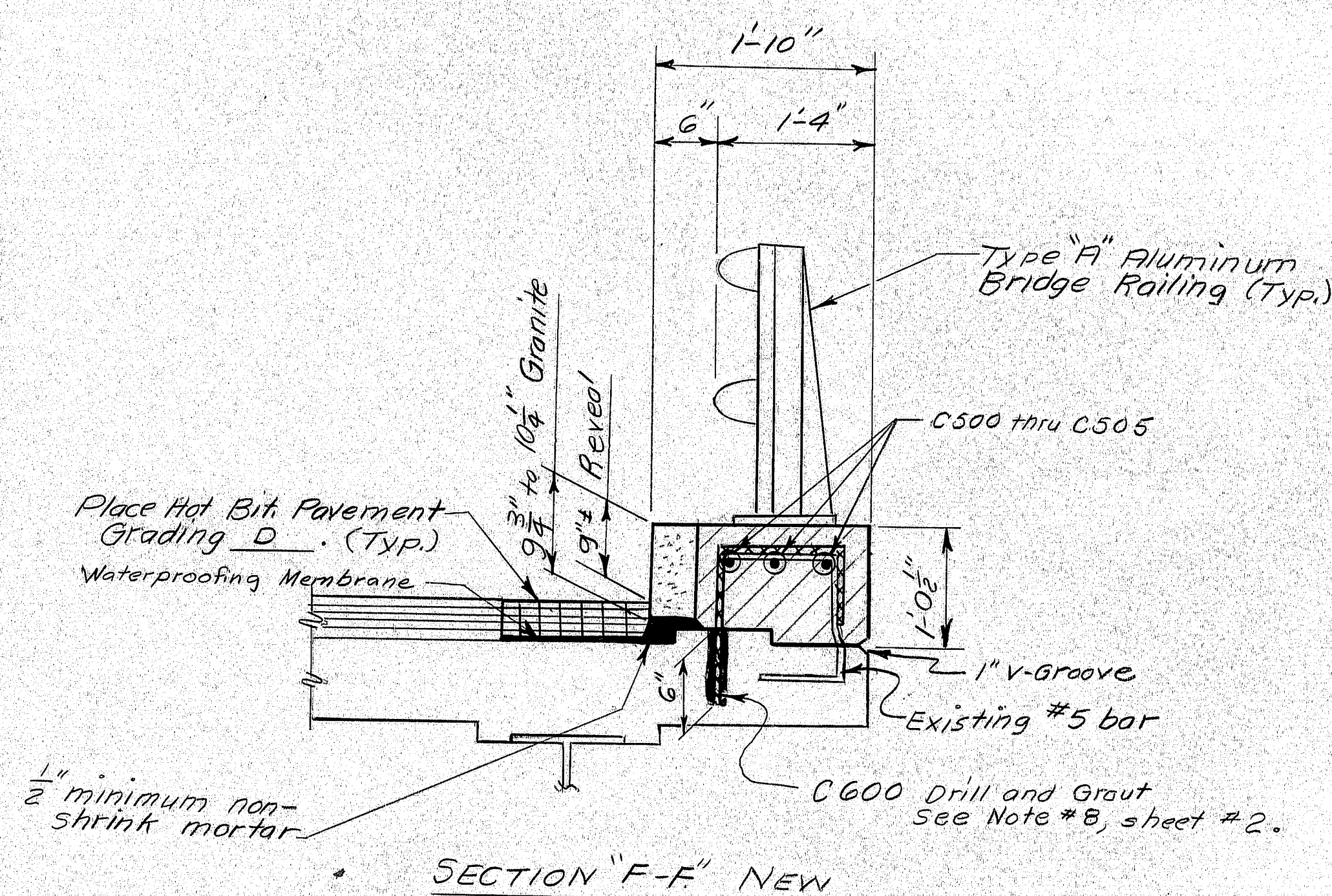
F.H.W.A. REQ. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	10	18

- Existing Concrete
- Existing Material to be removed
- New Concrete
- Existing Hot Bituminous Pavement
- New Hot Bituminous Pavement

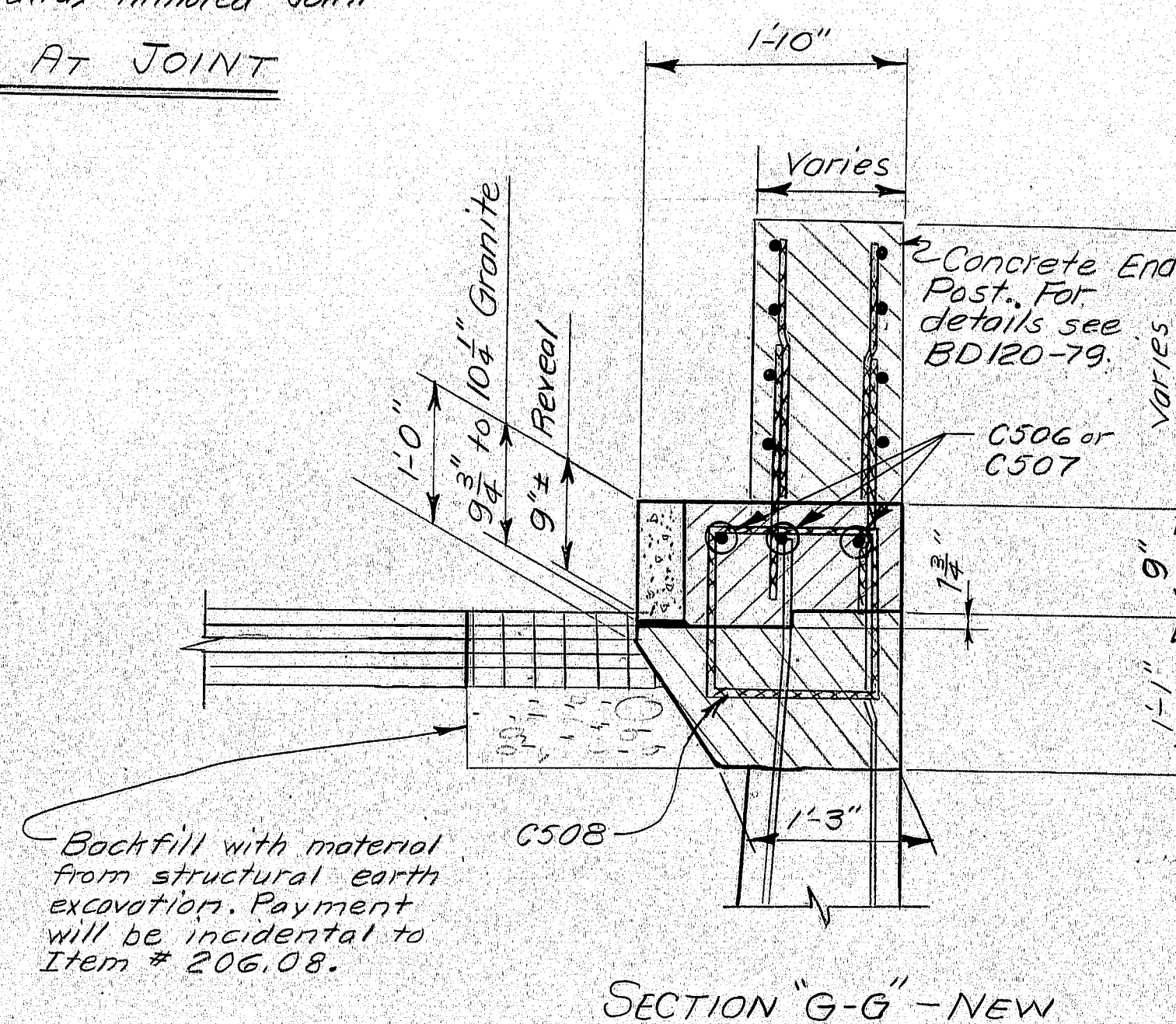
LEGEND OF SECTIONS



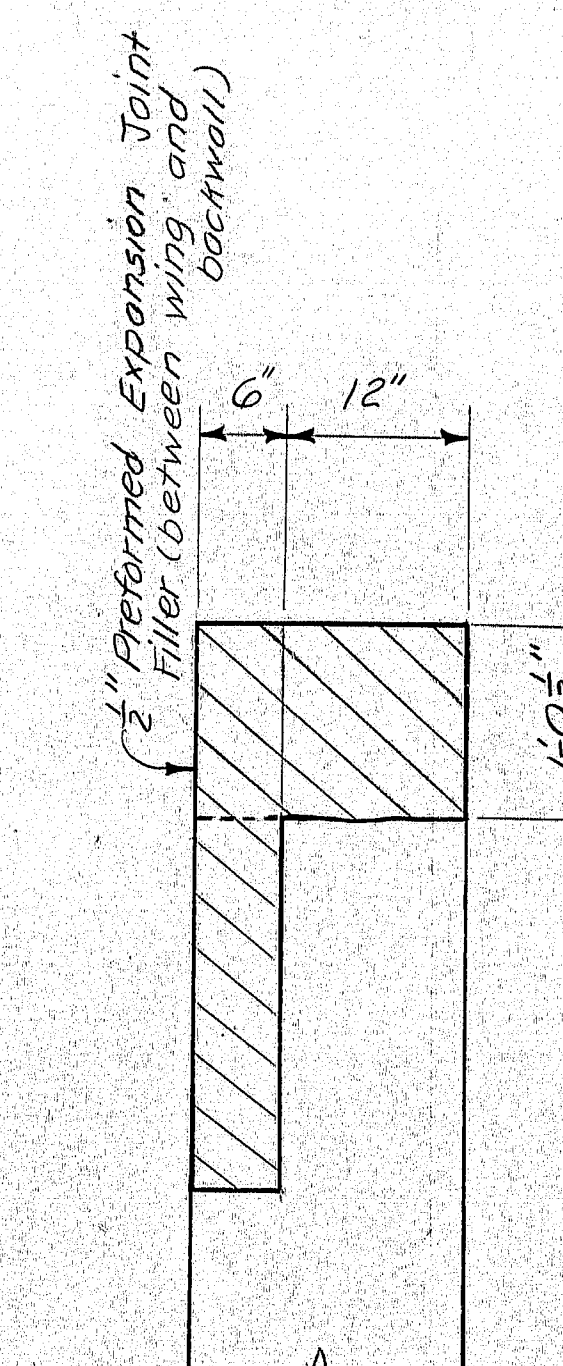
TYPICAL SECTION AT JOINT



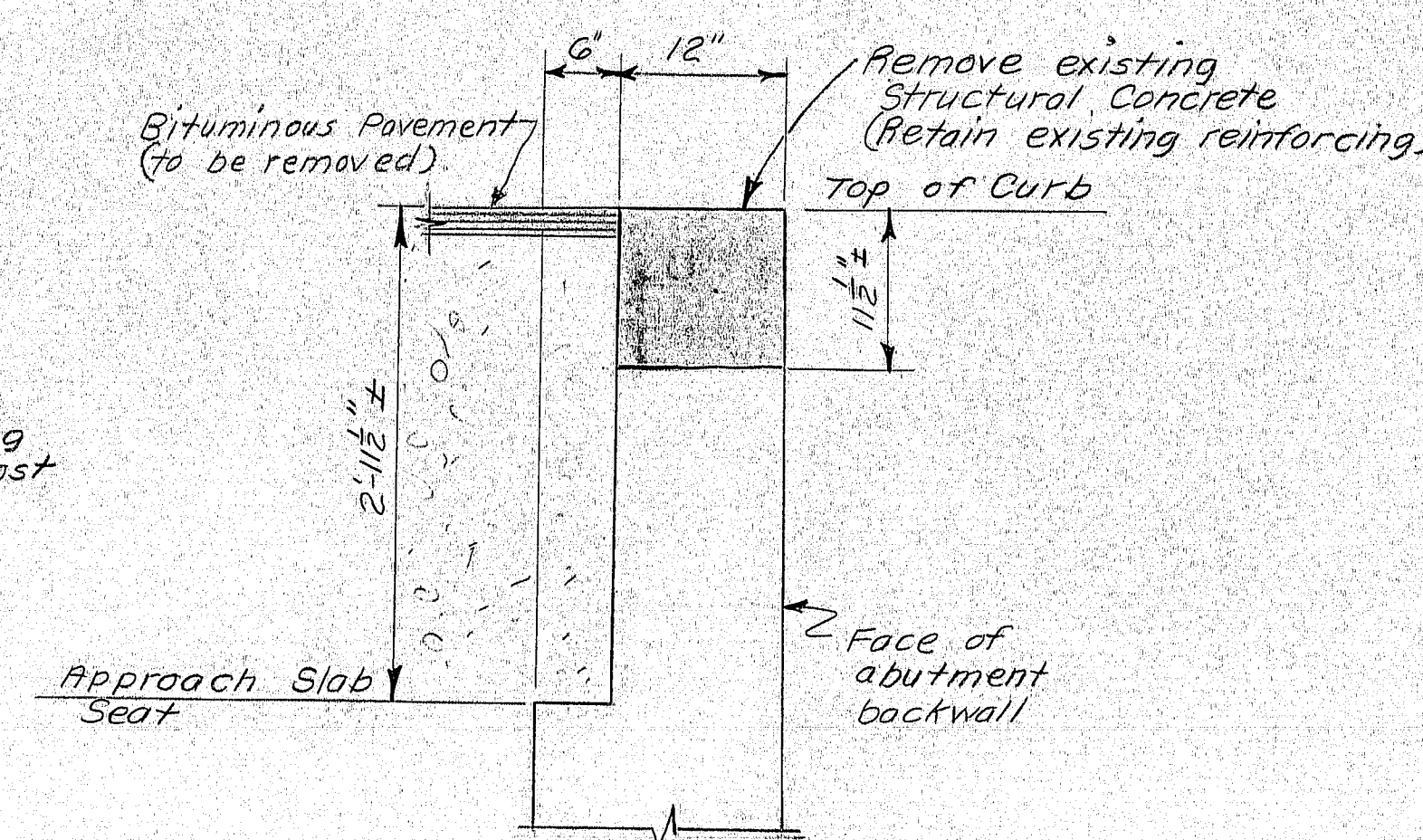
SECTION "F-F" NEW



SECTION "G-G" NEW



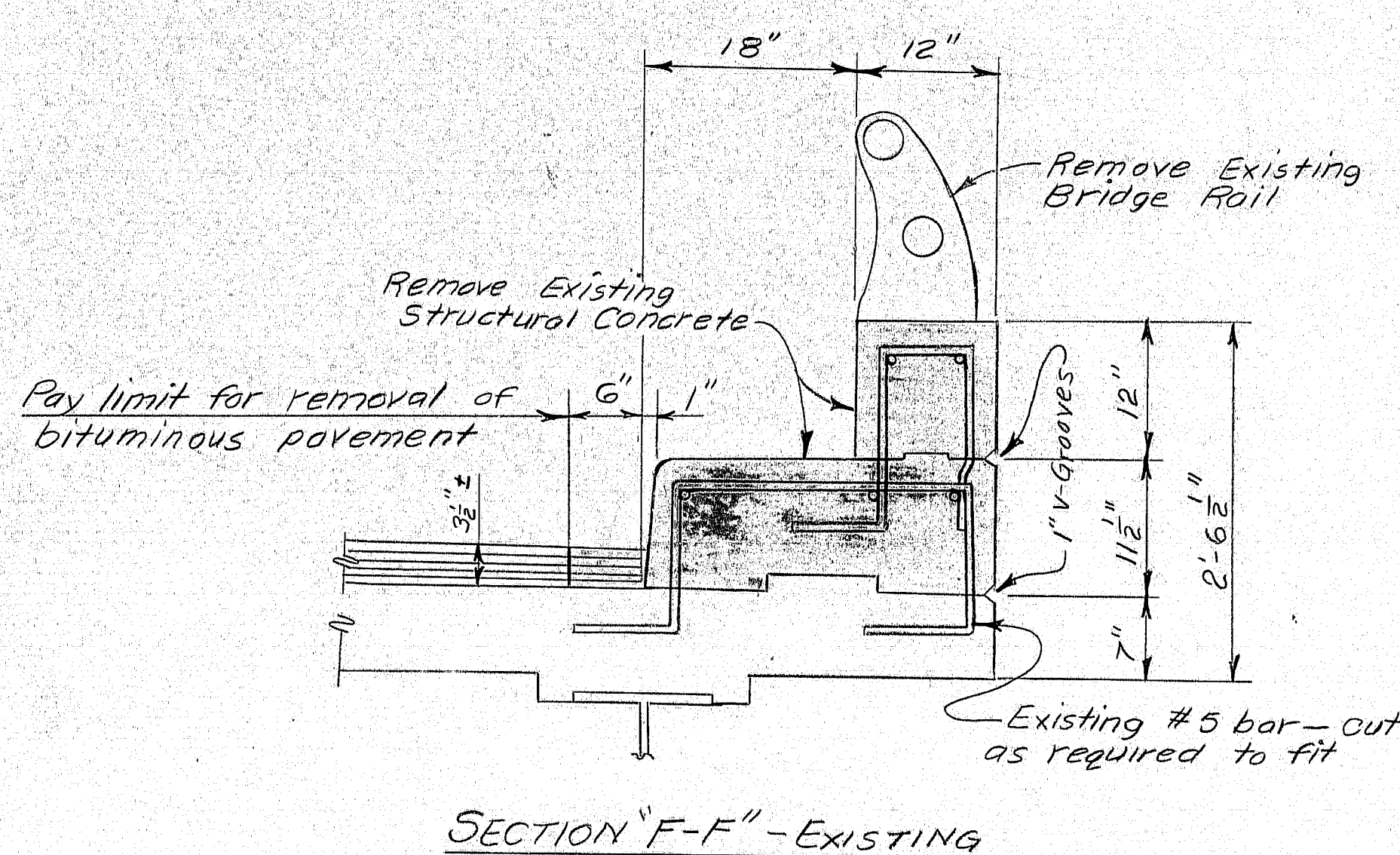
SECTION "H-H" NEW



SECTION "H-H" EXISTING

REFERENCES

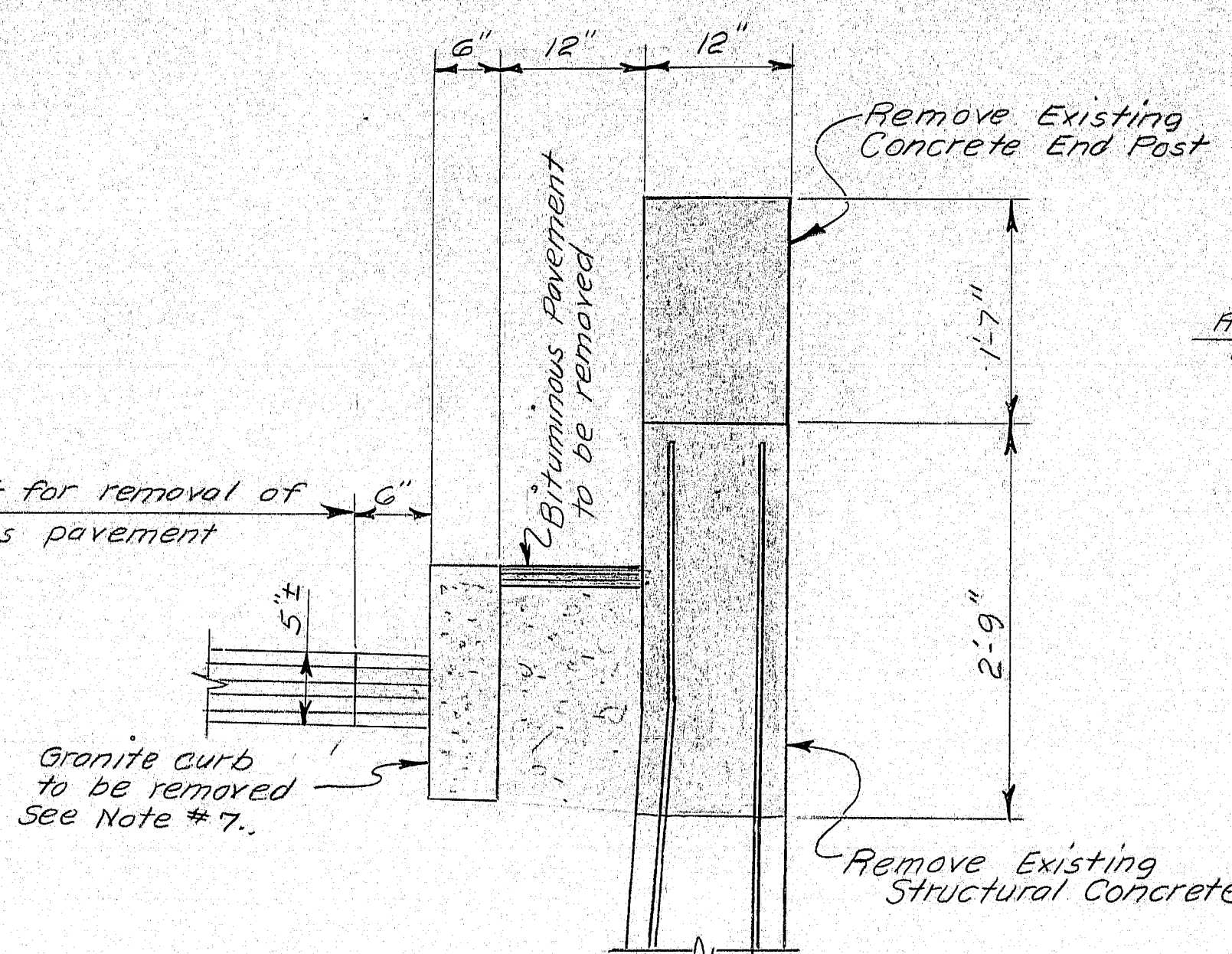
- Curbs and Railings — — — — — Sh #9.
- Concrete End Posts — BD120-79 — — — Sh #13.
- Aluminum Bridge Railing — BD114-77 — — — Sh #12.



SECTION "F-F" EXISTING

Pay limit for removal of bituminous pavement

Granite curb to be removed See Note #7.



SECTION "G-G" EXISTING

PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAIL	10-79
CHECKED	2/80
REVISIONS	
FIELD CHANGES	

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
INTERSTATE 95 OVER MESSALONSKEE STREAM & QUARRY ROAD IN THE CITY OF WATERVILLE KENNEBEC COUNTY
CURB SECTIONS
SHEET 10 OF 18 AUGUSTA, MAINE Nov. 1979

172-124

REINFORCING STEEL SCHEDULE																											
STRAIGHT BARS												BENT BARS															
MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	LOCATION	MARK	NO.	LENGTH	TYPE	A	B	C	D	E	F	G	H	O	R	LOCATION	
			I-95 over Bond Brook and Mount Vernon Avenue				I-95 over M.C.R.R. and County Road				I-95 over Messalonskee Stream and Quarry Road																
A500	84	60'-0"	longitudinal	B500	48	40'-0"	longitudinal	C500	156	60'-0"	longitudinal	C508	96	5'-4"	H	6"	1'-1"	1'-1"	1'-1"	1'-1"			6"				Transverse
A501	24	16'-8"	do	B501	24	31'-3"	do	C501	12	26'-8"	do	C600	4088	3'-0"	S		1'-3"	1'-1"	8"							do	
				B502	12	14'-4"	do	C502	72	26'-5"	do																
				B503	12	32'-4"	do	C503	12	25'-8"	do																
								C504	12	27'-5"	do																
								C505	24	12'-1"	do																
								C506	6	18'-2"	do	B600	1044	3'-0"	S		1'-3"	1'-1"	8"							Transverse	
								C507	6	28'-2"	do																

FHWA REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	11	18

All dimensions are out to out of reinf. bar

Bending details and hooks shall conform to the recommendations of ACI Standard 315-65.

Reinforcing Bar: ASTM A615 Grade 60

GENERAL NOTES

1. First digit(s) following the letter of the Mark indicates size of reinforcement.

Mark (4 502)	bar size - #5
Mark (P 1001)	bar size - #10
Mark (S 603)	bar size - #6
2. Letter of Marks A, P & S locates bars of Abutments, Piers, and Superstructure parts respectively.
3. Abbreviations :
do ~ ditto

Revised as Built - D. Sherlock, 1-26-81

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95
OVER
BOND BROOK
COUNTY ROAD
MESSALONSKEE STREAM
IN THE CITIES OF
AUGUSTA & WATerville
REINFORCING STEEL SCHEDULE
SHEET 11 OF 18 AUGUSTA, MAINE Nov. 19

172-125

PLANS	DESIGN - DETAIL	BY	DATE
	CHECKED		11/79
	REVISIONS	G.O.F.	2/80
	FIELD CHANGES		

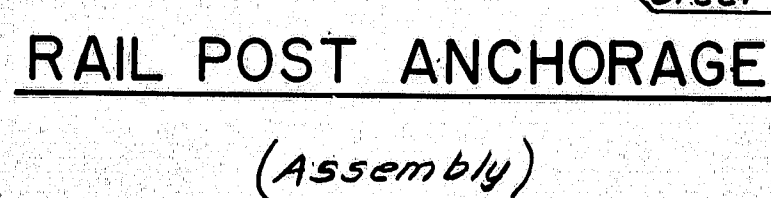
DESIGN SPECIFICATIONS
AASHTO Standard Specifications
for Highway Bridges 1973, and
Interims 1974, '75, '76, '77



Lengths of rail shall be attached to a minimum of four (4) rail posts wherever possible, and in any case never less than two (2). Rail posts are to be set normal to grade unless otherwise shown on the Bridge Plans.



* Preferable minimum dimensions. For actual dimensions see Bridge Plan.	\$ Anchor Bolts	\$ Anchor Bolts
---	-----------------	-----------------



If cut threads are used, body diameter shall be not less than nominal diameter.
If rolled threads are used, body diameter shall be not less than root diameter of the threads.



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

STANDARD DETAILS
(BD 114 - 77)

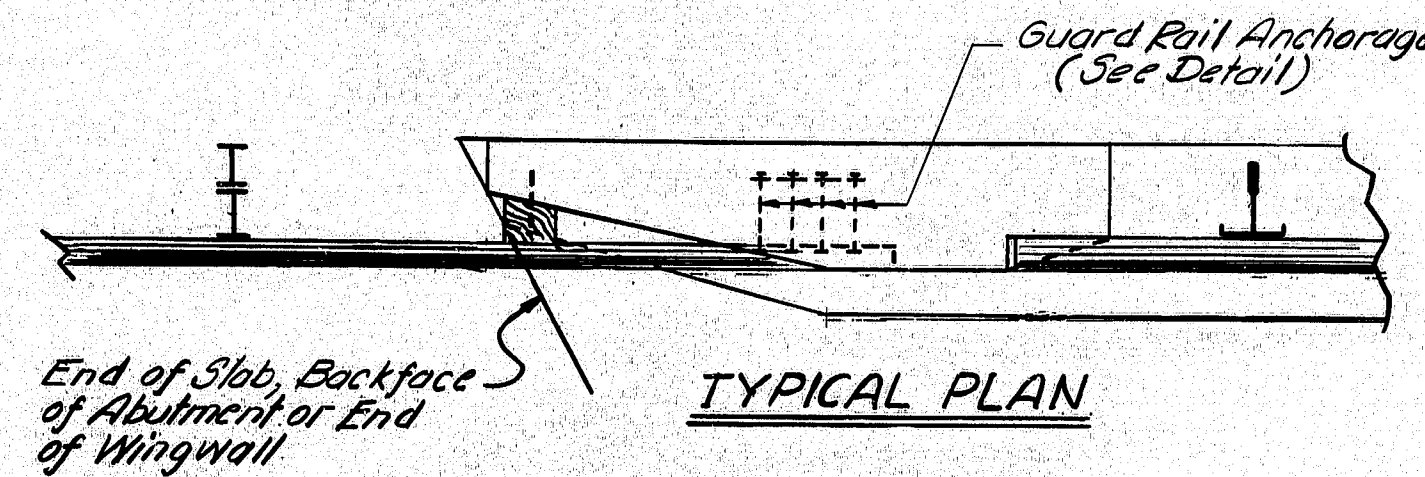
ALUMINUM BRIDGE RAILING

2 - BAR (SEMI-ELLIPSE)
TYPE "A"

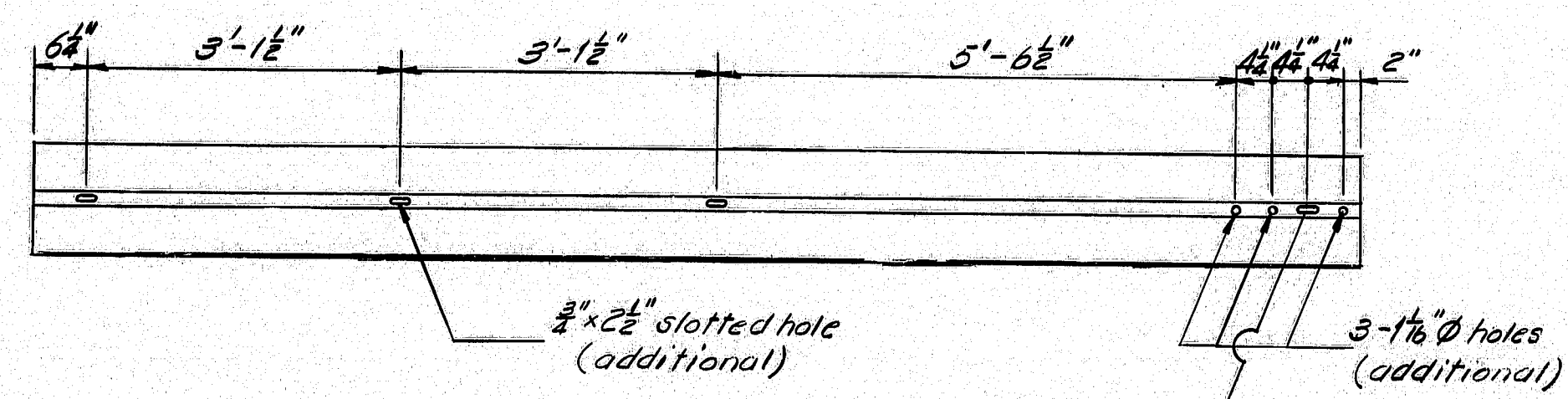
SHEET 2 OF 8 AUGUSTA, MAINE DEC. 1977

PLANS	DESIGN - DETAILED	BY	DATE
	CHECKED	<i>K. Jacobs, K. Leach</i>	<i>Jan 1997</i>
	REVISIONS		
	FIELD CHANGES	<i>M. Peterson</i>	<i>Jan 1998</i>

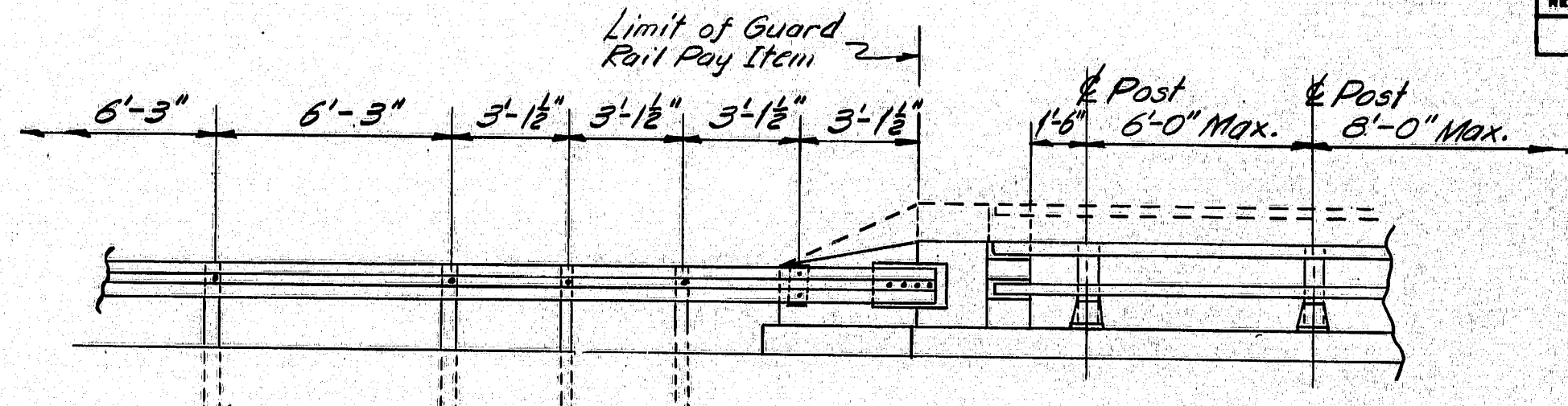
R.R.W.A. No.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	13	18



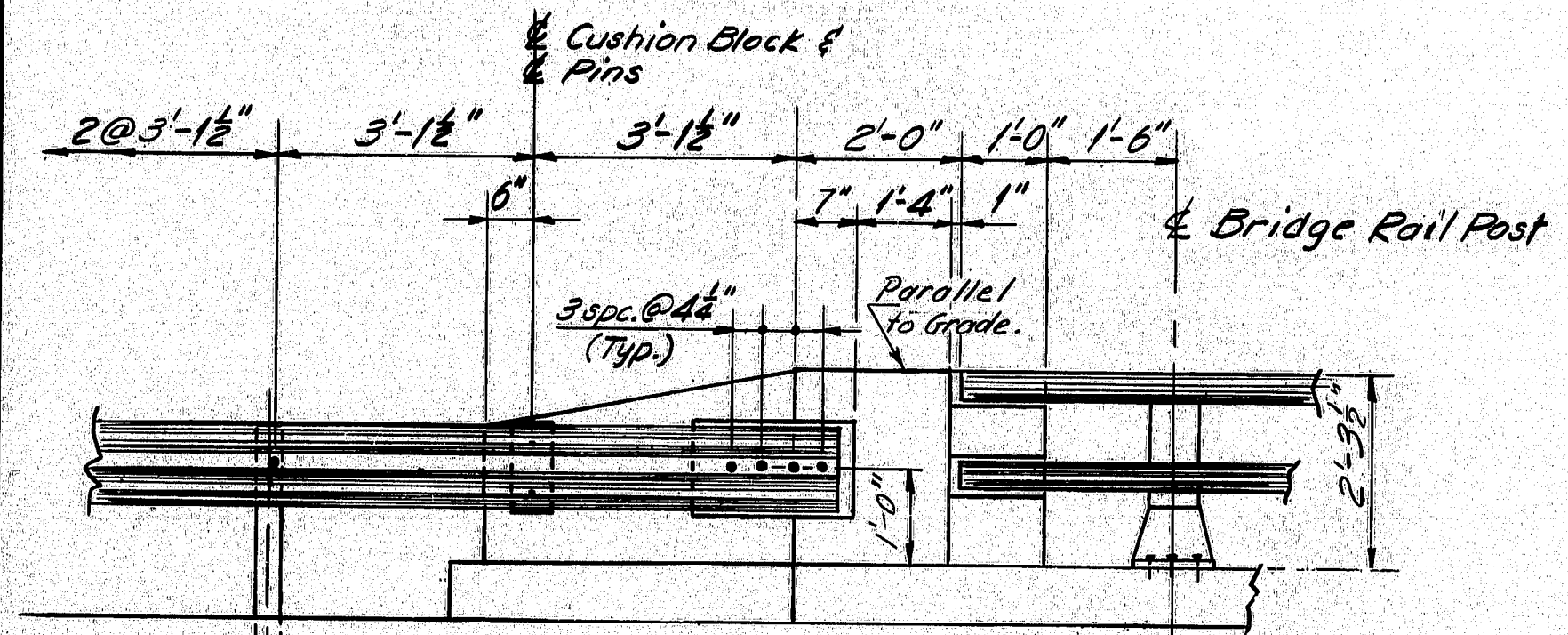
TYPICAL PLAN



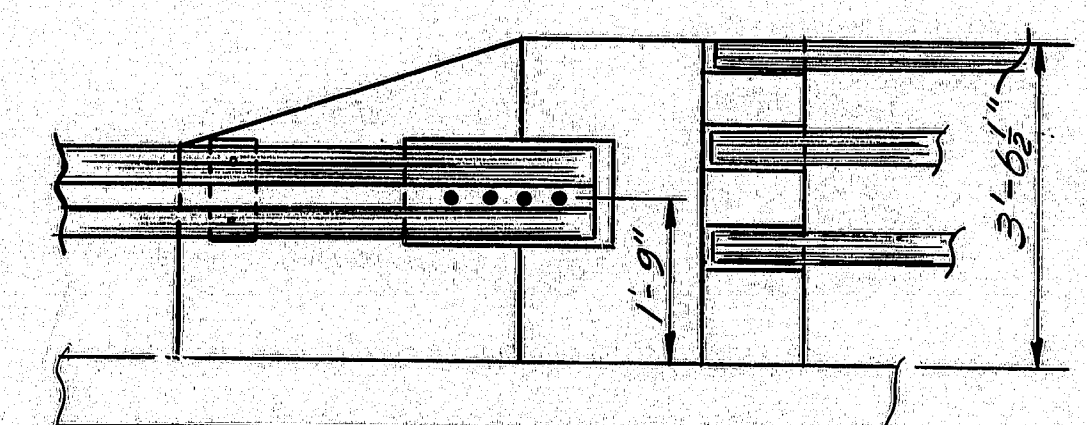
MODIFIED GUARD RAIL SECTION
See Note #5



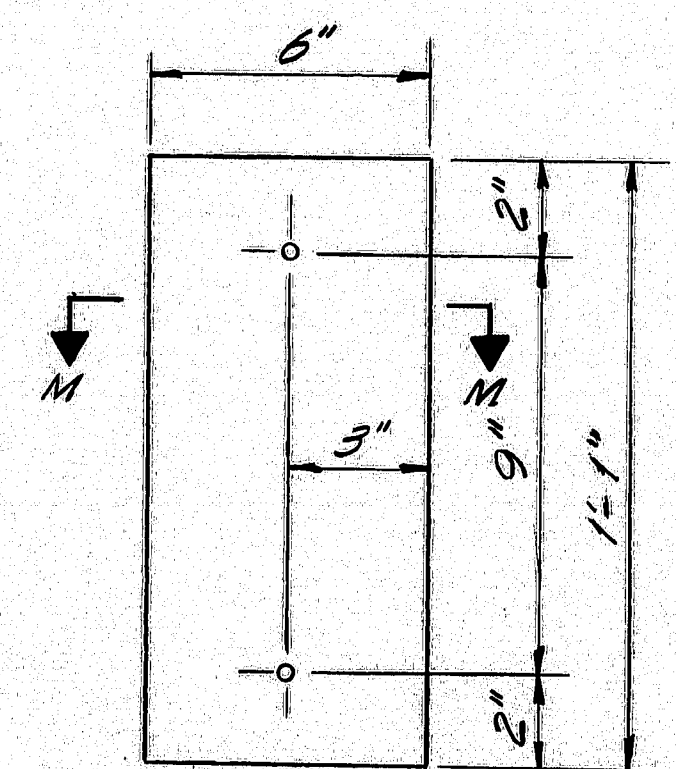
RAILING - ELEVATION



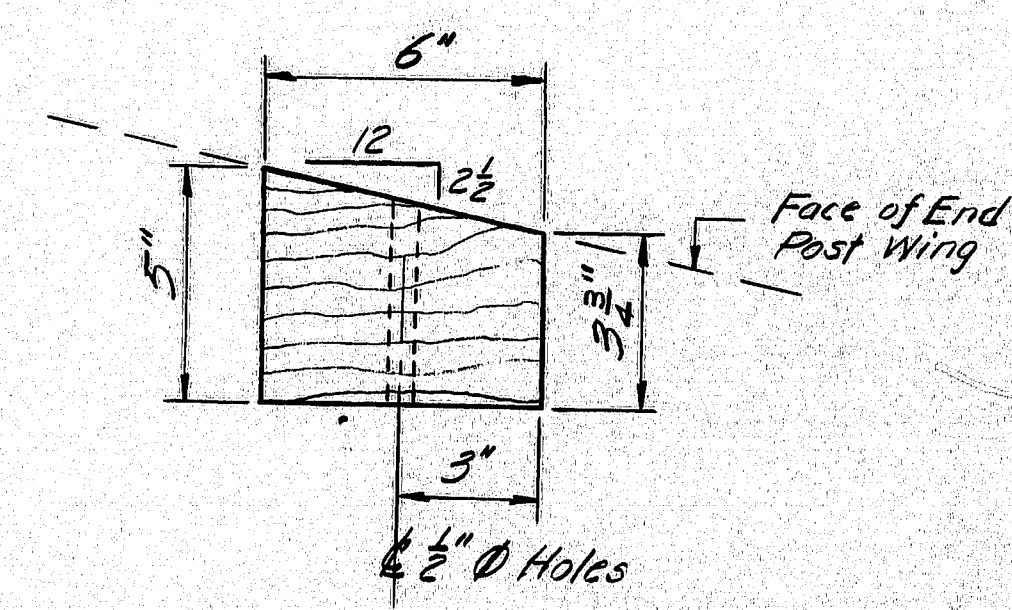
ELEVATION
2-Bar Bridge Rail (Aluminum or Steel)



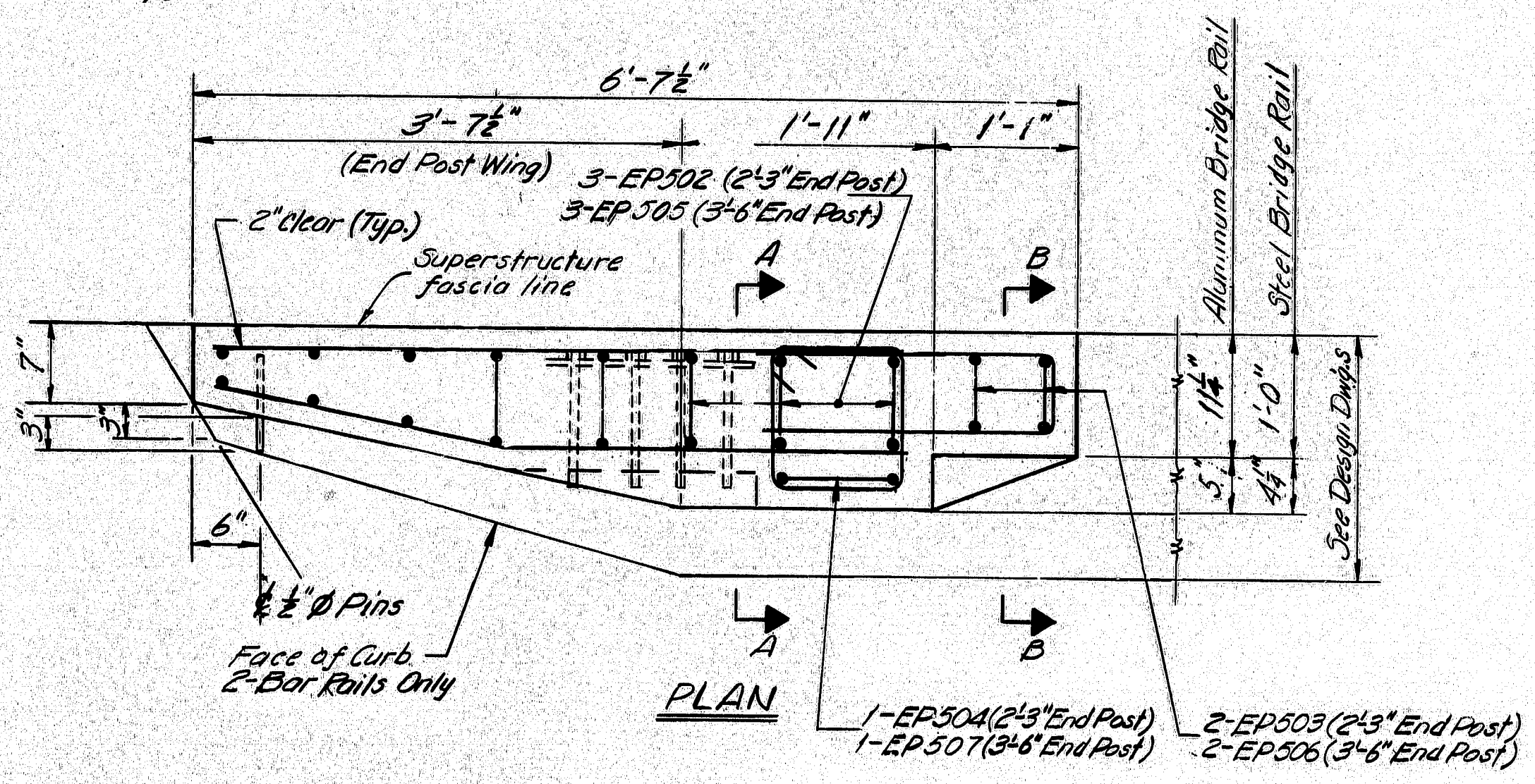
ELEVATION
3-Bar Bridge Rail (Aluminum or Steel)



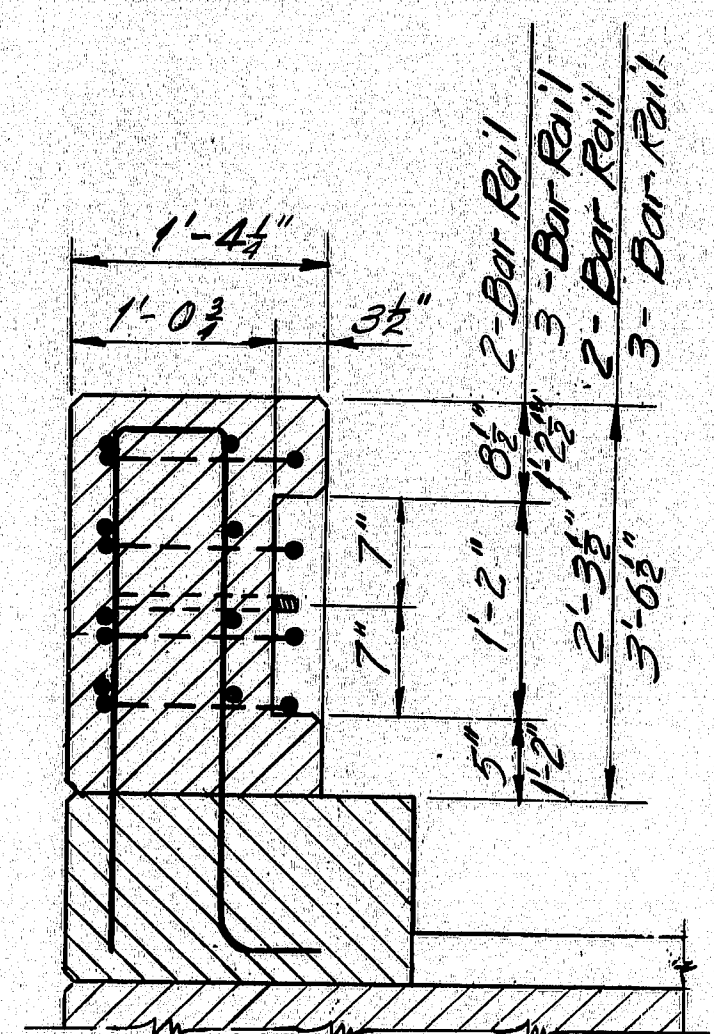
CUSHION BLOCK
(See Note #6)



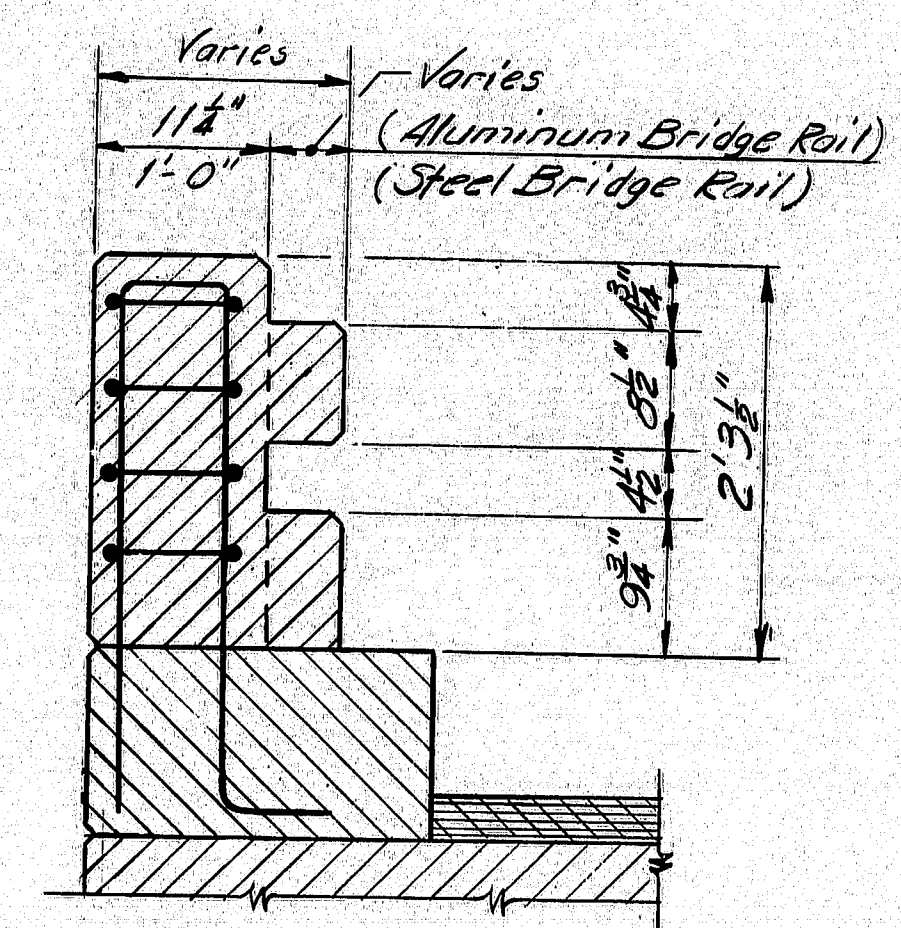
SECTION M-M



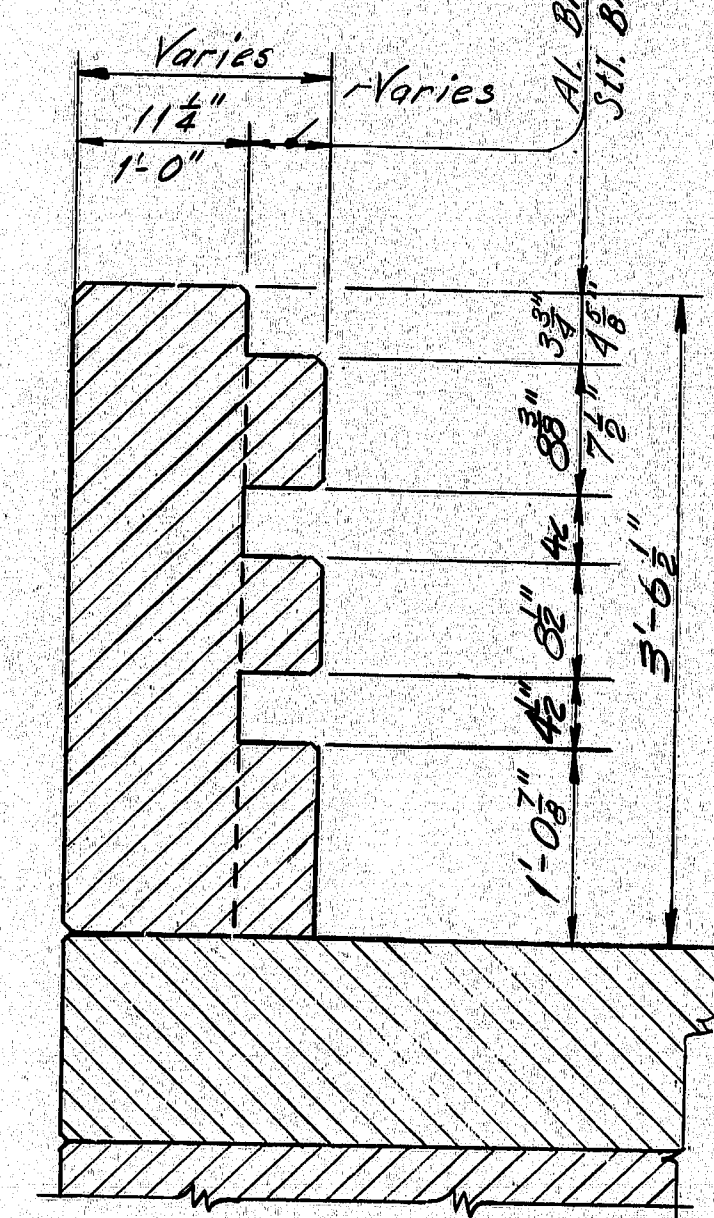
PLAN



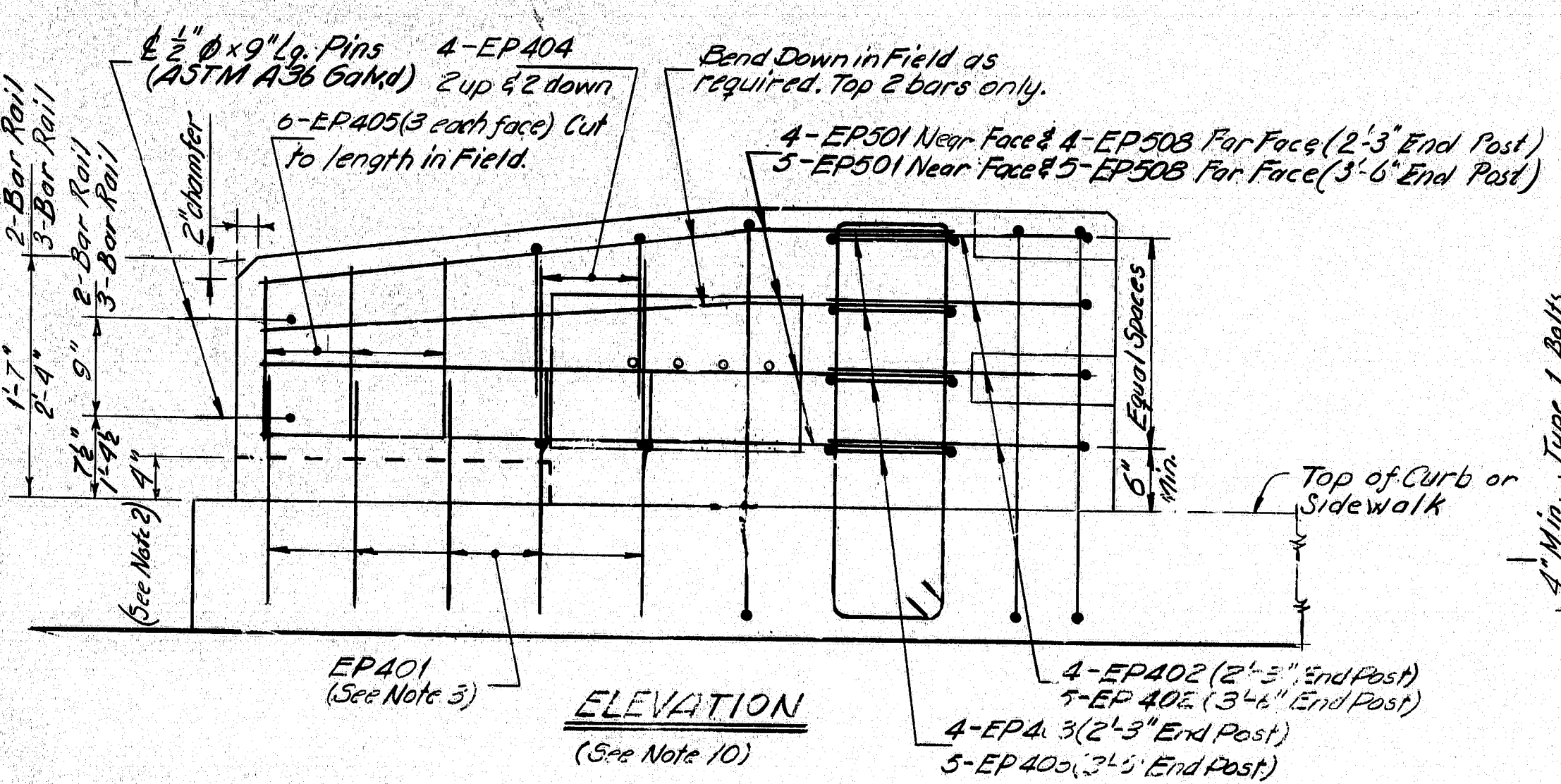
SECTION A-A



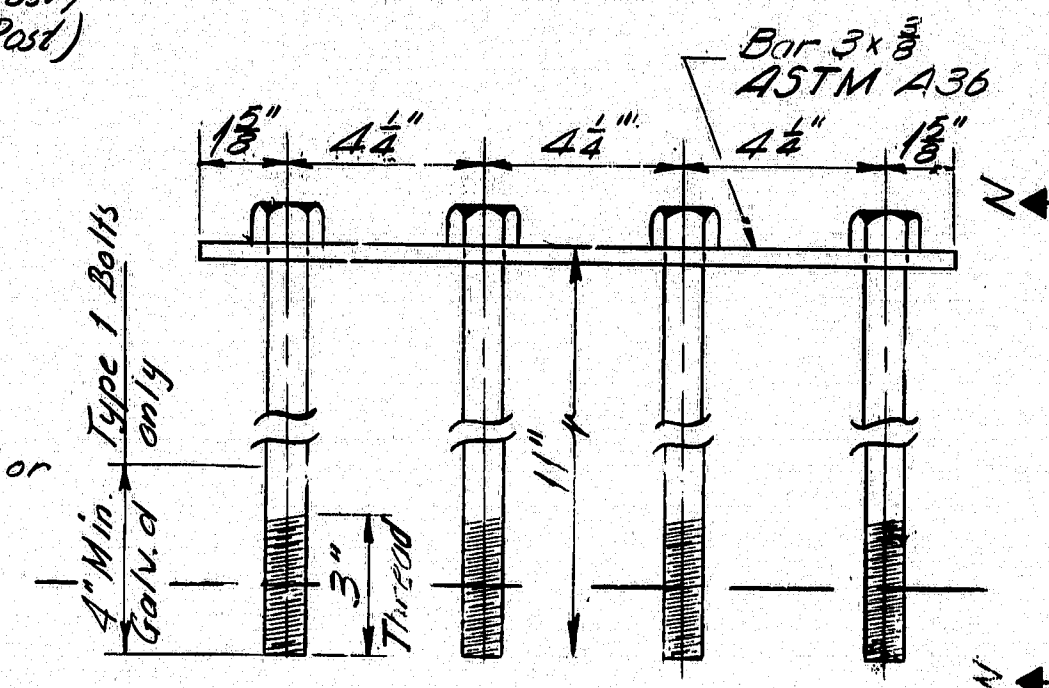
SECTION B-B
2-Bar Rail
(Aluminum or Steel)



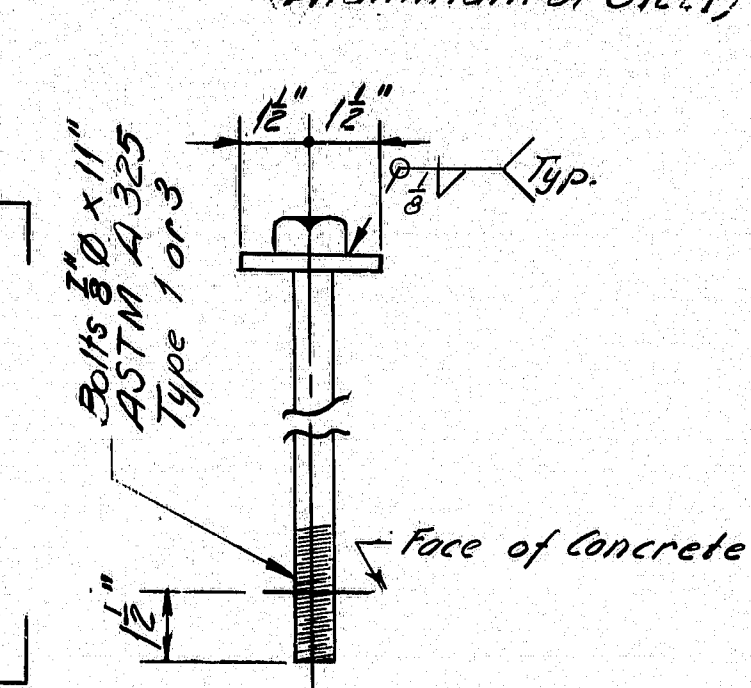
SECTION C-C
3-Bar Rail
(Aluminum or Steel)



ELEVATION
(See Note 10)



DETAIL
GUARD RAIL ANCHOR



VIEW N-N

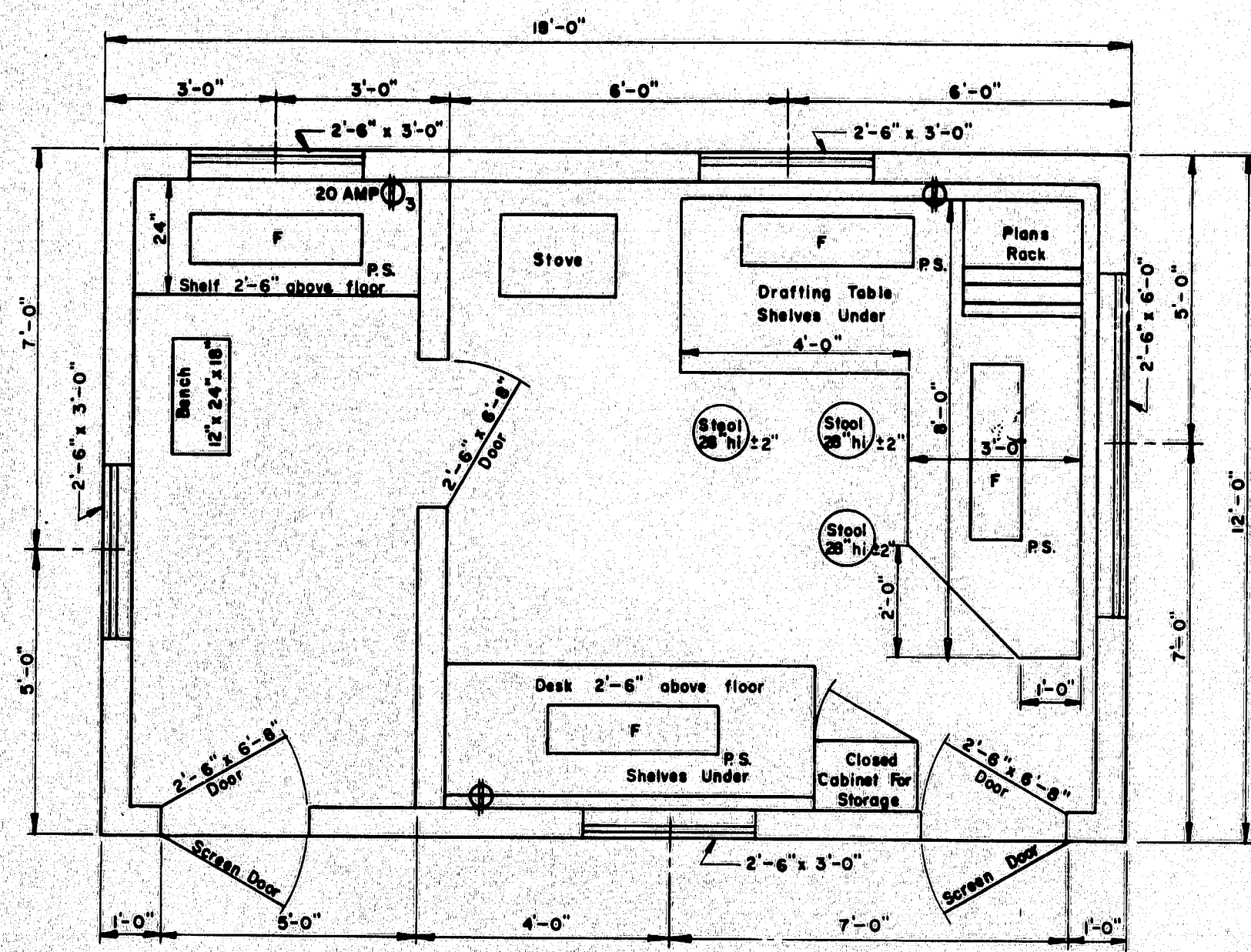
NOTES

- 1.) For locations of the end posts on the structure see design drawings.
- 2.) At times an End Post Wing may be cantilevered for all or part of its length. For details see design drawings.
- 3.) If End Post Wing is cantilevered bars EP 401 to be omitted as needed in cantilevered section.
- 4.) Nuts for 1/2" anchor bolts to be incidental to guard rail pay items. Nuts shall conform to A.S.T.M.-A563, Grade DH, galvanized in accordance with A.S.T.M. A153, or Grade C3, plain.
- 5.) Additional holes in the Modified Guard Rail Section shall be made by drilling, punching, or any other method that produces a neat, clean hole of the required size. Burning of holes will not be allowed.
- 6.) Cushion Block material shall be as specified in subsection 710.07, paragraph b & c and treated in accordance with the provisions of subsection 606.03(b) of the standard specifications. Payment to be incidental to Guard Rail pay item.
- 7.) After installation of guard rail is complete upset the thread on the anchor bolts in three places around each bolt, at the junction of the nut and the exposed thread, with a center punch or similar tool.
- 8.) Guard Rail Anchorage to be incidental to the applicable concrete pay item.
- 9.) End Posts to be constructed normal to grade, unless otherwise shown on the plans.
- 10.) When End Post Wing is cantilevered more than 2'-0" all number 5 bars shall be replaced with number 7 bars.

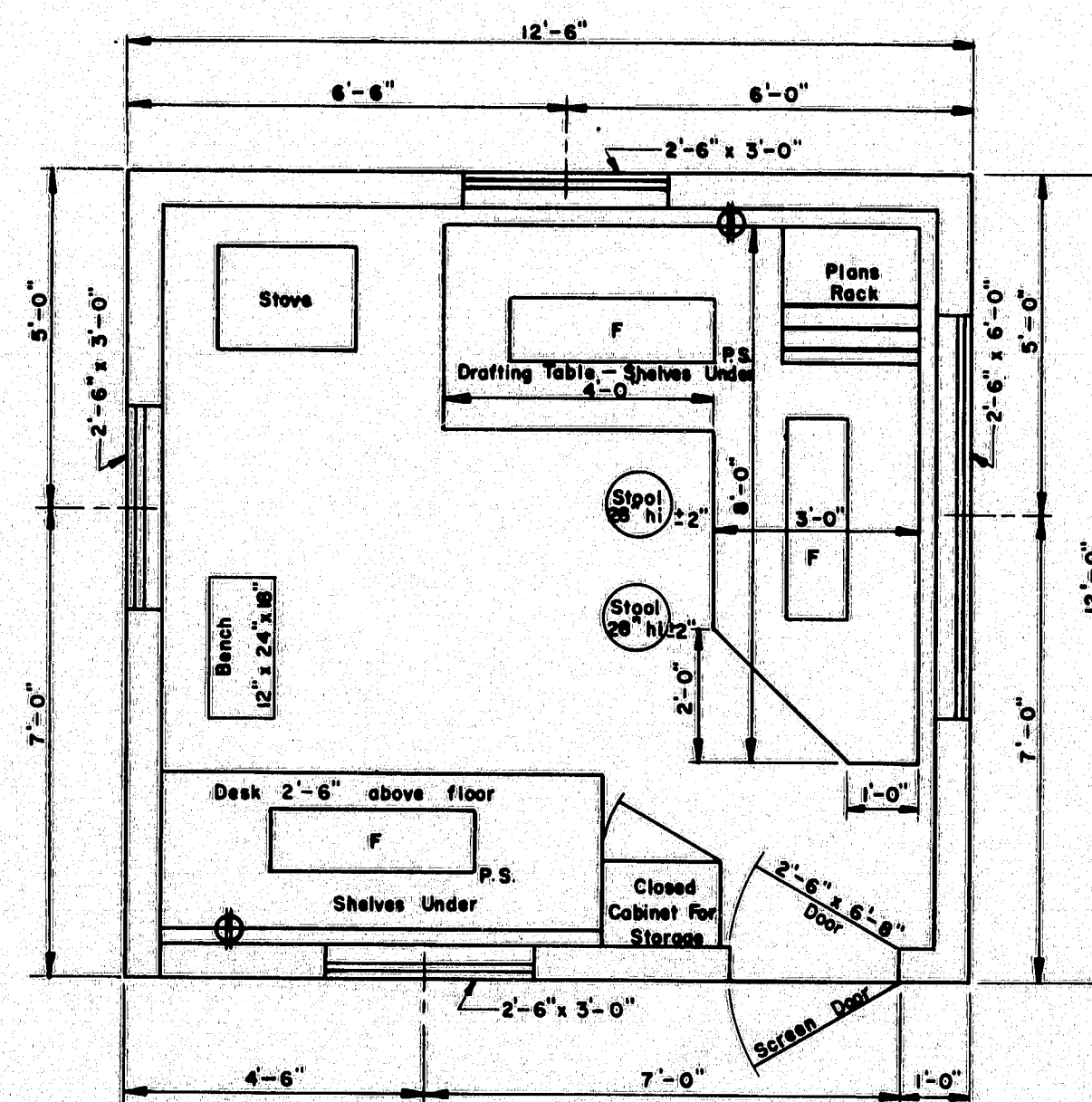
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	W.T.	6/14/94
CHECKED	H. Jacobs	6/14/94
REVISIONS		
FIELD CHANGES		

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

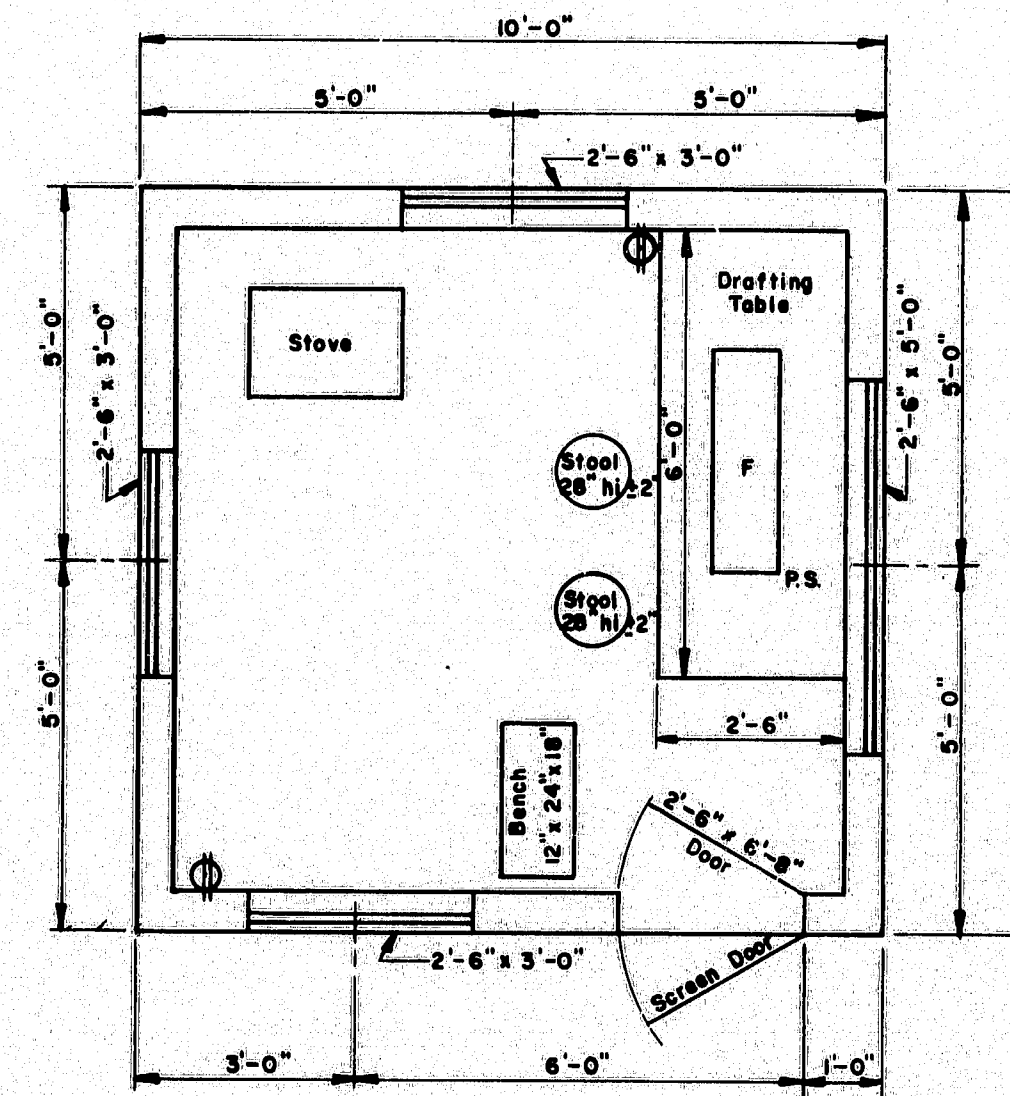
STANDARD DETAILS (BD 120-79) CONCRETE END POSTS



FLOOR PLAN
TYPE "A"

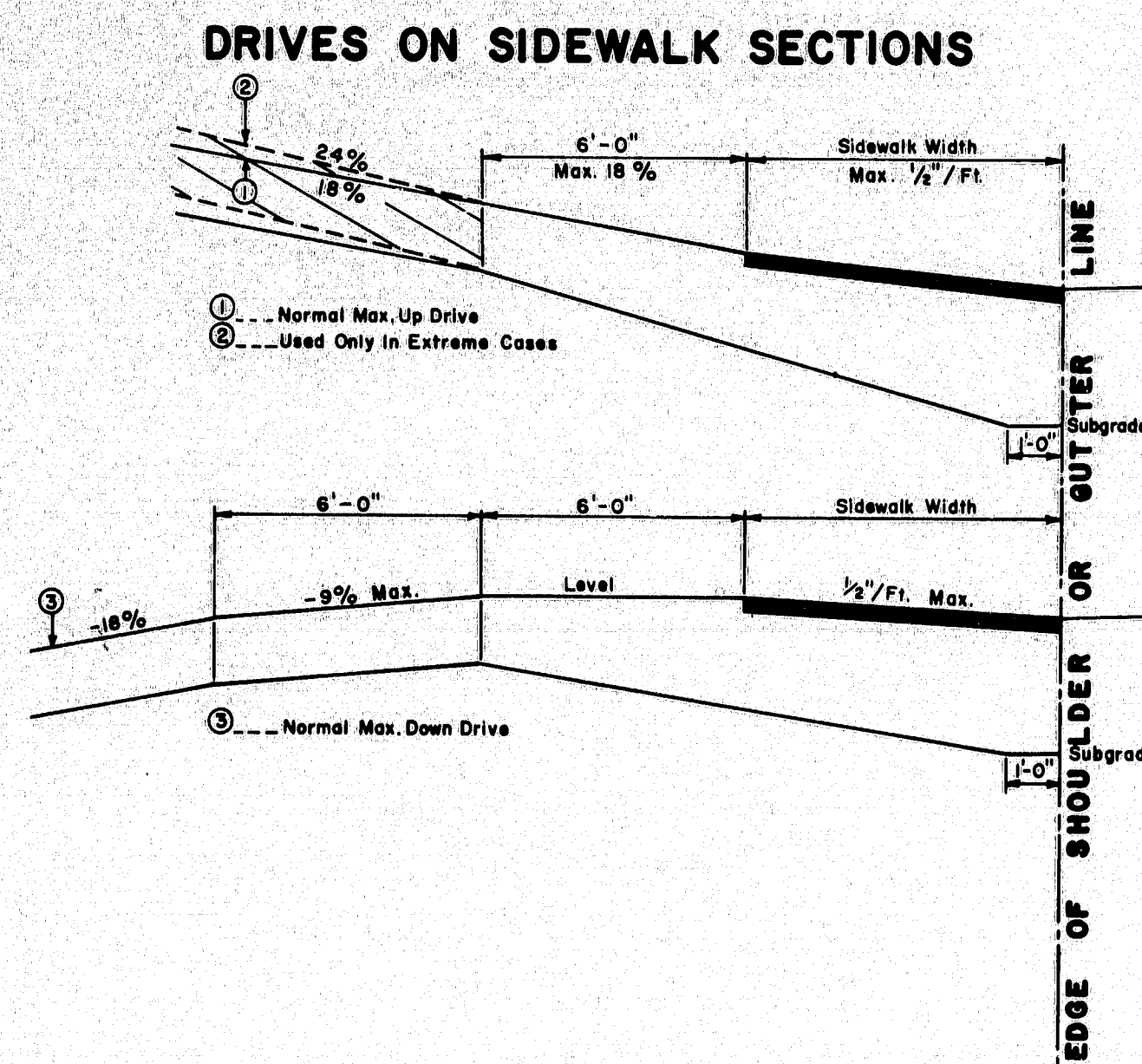


FLOOR PLAN
TYPE "B"

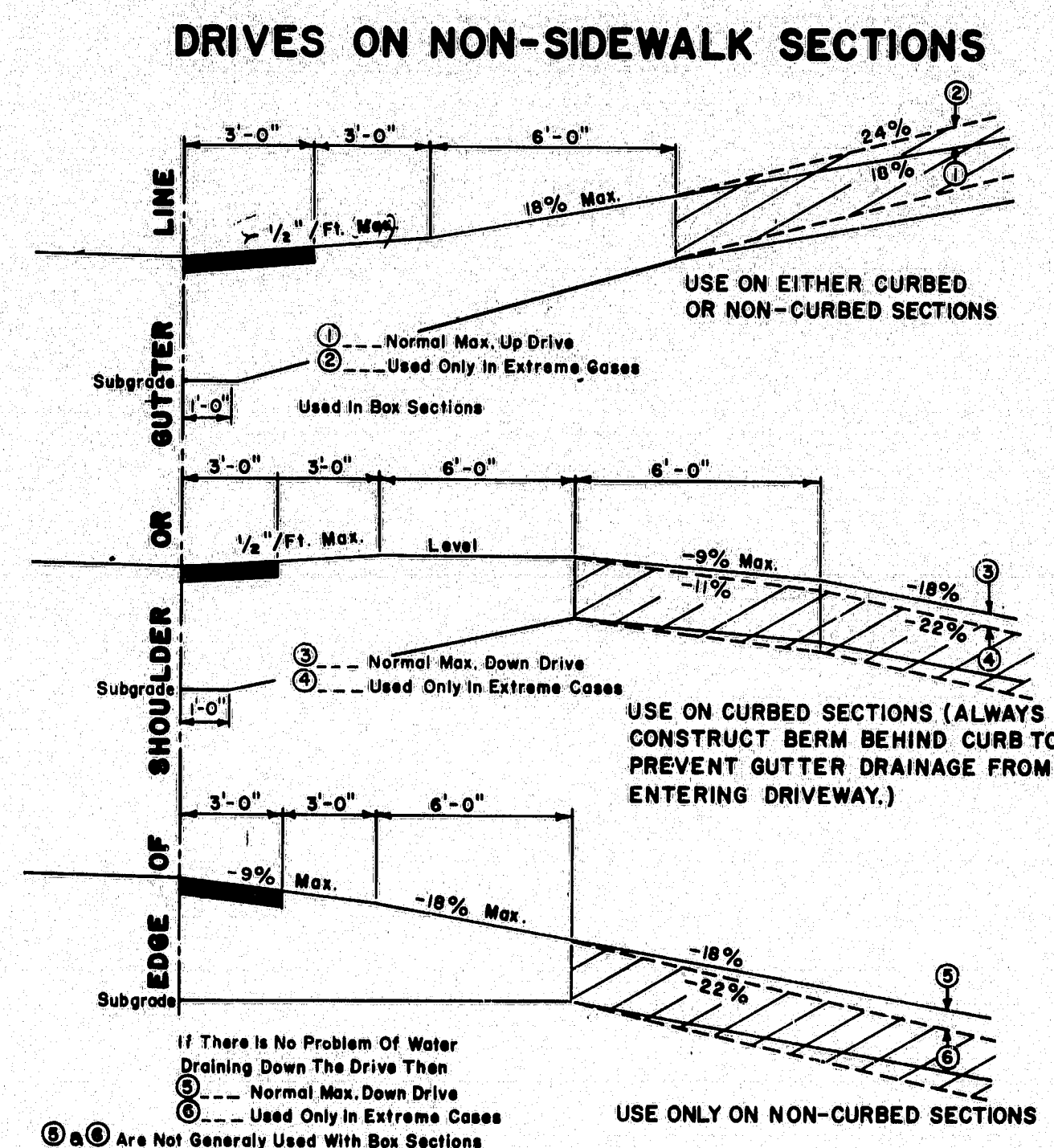


FLOOR PLAN
TYPE "C"

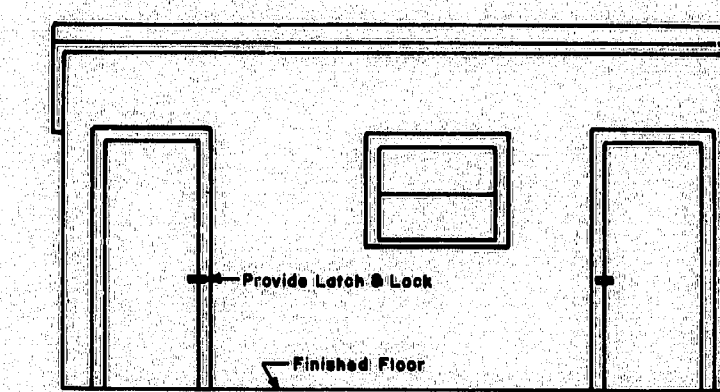
- GENERAL NOTES**
- Drafting table shall be 3'-4" high at front edge and placed 2" from studs to allow prints to hang down behind table when in use.
 - Shelves under desk shall be constructed to receive 1 1/2" x 14" x 25" transfiles.
 - Windows shall be double hung.
 - Stovepipe shall not be in direct contact with combustible material; the pipe shall be surrounded with at least 6" of fireproof material.
 - Continuous 110 volt 60 cycle electric service shall be supplied.
 - The engineer may rearrange the items shown on the plan views during construction of the field office.
 - FURNISHINGS TO BE SUPPLIED:**
 - 2 Straight back chairs for types A and B
 - 1 Bench for types A, B & C
 - 3 Stool for type A
 - 2 Stools for types B & C
 - SYMBOLS:**
 - F: Fluorescent lights (2 light, rapid start 48" strips and 40 watt bulbs.)
 - P.S.: Pull switch
 - ⊕: Duplex wall outlet - 15 amp unless otherwise noted
 - ⊕₃: Triplex Wall Outlet
 - For the Type "A" Field Office one clean 55 gal. drum shall be supplied, installed on a suitable rack and equipped with a spigot suitable for drawing off water. The drum shall be furnished with water at all times.



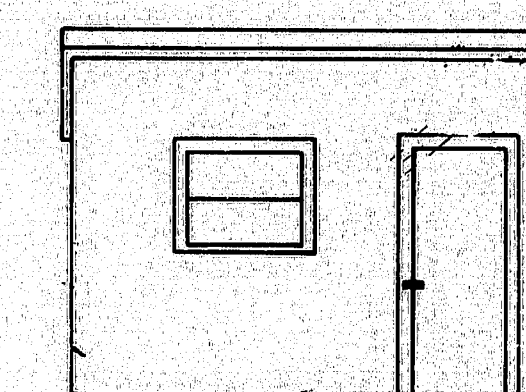
- GENERAL NOTES**
- The sidewalk width shall be paved in all cases.
 - All residential or commercial drives 10% and over shall be paved.
- NOTES ON MAXIMUM DRIVEWAY PROFILES**
- These profiles are a guide for the majority of cases, but should be field checked when the main line grade is steep (4% to 6% or greater) or the angle of approach to the drive is unusual.
 - Generally the majority of drives on a project will be built with flatter profiles than these maximum cases.
 - When grading drives which are flatter than the maximum profiles the following rule of thumb should be used, do not exceed a grade % change of more than 9% in a 6 foot increment of driveway length. This applies to both up and down profiles.



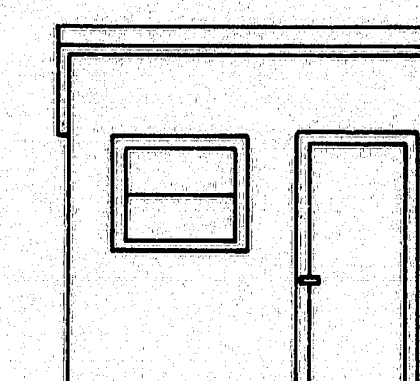
- GENERAL NOTES**
- The first 3' shown as pavement shall be paved only when abutting a paved area.
 - All residential or commercial drives 10% and over shall be paved.
- NOTES ON MAXIMUM DRIVEWAY PROFILES**
- These profiles are a guide for the majority of cases, but should be field checked when the main line grade is steep (4% to 6% or greater) or the angle of approach to the drive is unusual.
 - Generally the majority of drives on a project will be built with flatter profiles than these maximum cases.
 - When grading drives which are flatter than the maximum profiles the following rule of thumb should be used, do not exceed a grade % change of more than 9% in a 6 foot increment of driveway length. This applies to both up and down profiles.



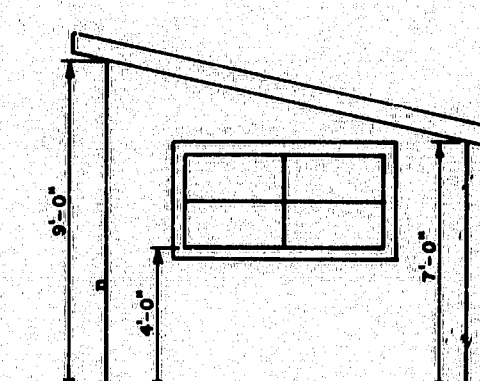
FRONT ELEVATION
TYPE "A"



FRONT ELEVATION
TYPE "B"



FRONT ELEVATION
TYPE "C"



SIDE ELEVATION
TYPES "A" "B" & "C"

REVISIONS	
PLATE	"D" E
	3-16-73

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
AUGUSTA, MAINE

STANDARD DETAILS

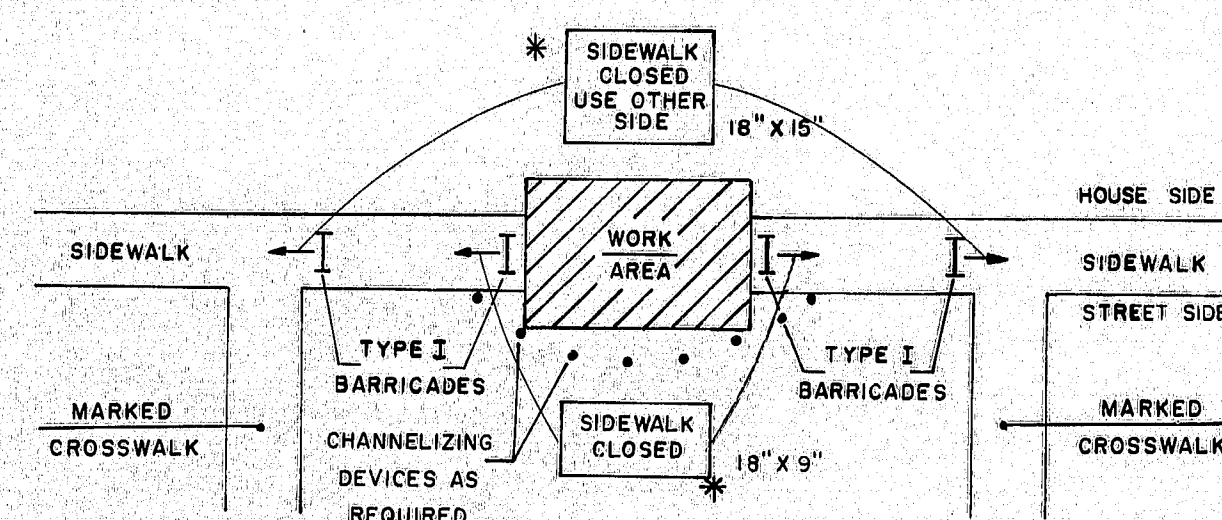
DRIVEWAY DETAILS
FIELD OFFICES
TESTING LABORATORY

Sh. 15 of 18 AUG. 1969

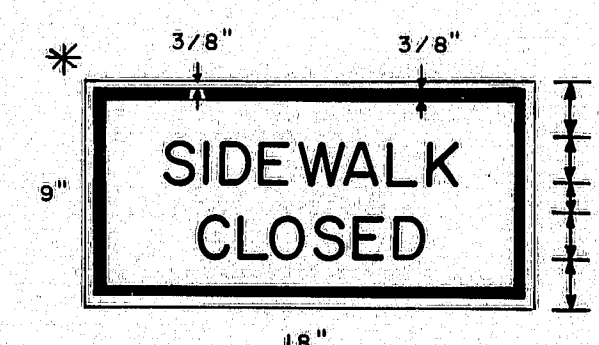
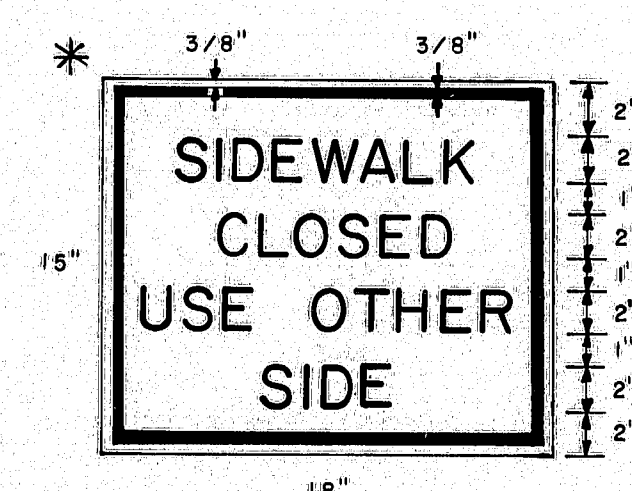
172-129

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	16	18

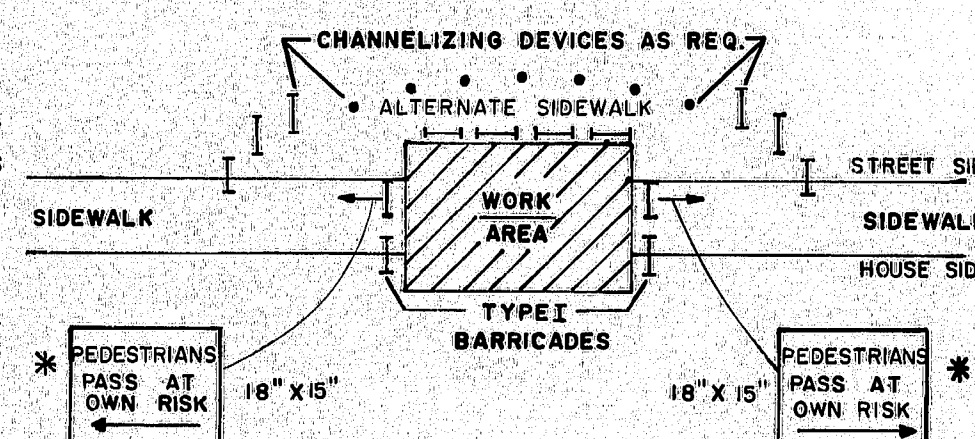
* NON-REFLECTORIZED WHITE BACKGROUND, BLACK TEXT
AND BORDER-2" SERIES C UPPER CASE LETTERS



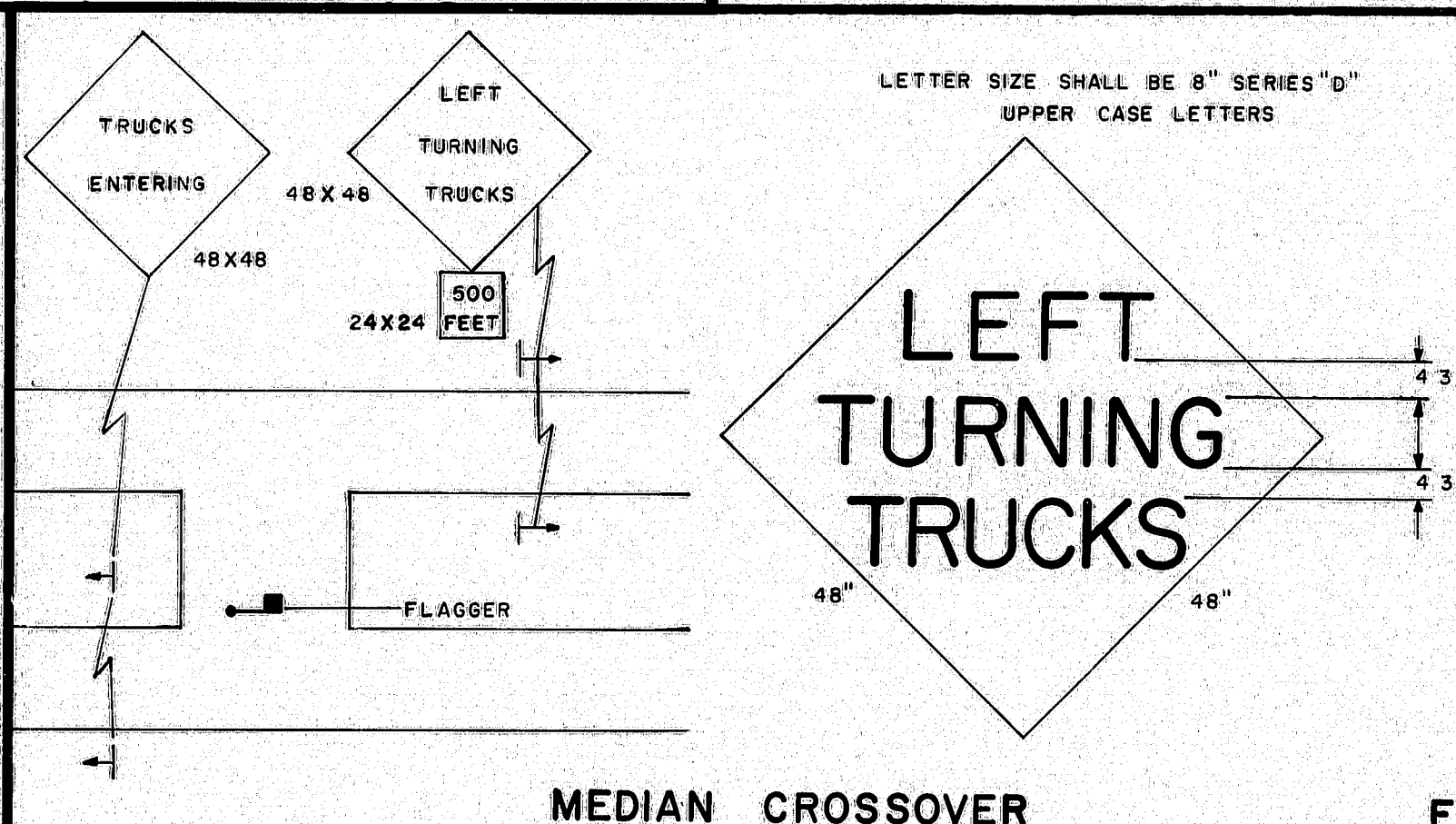
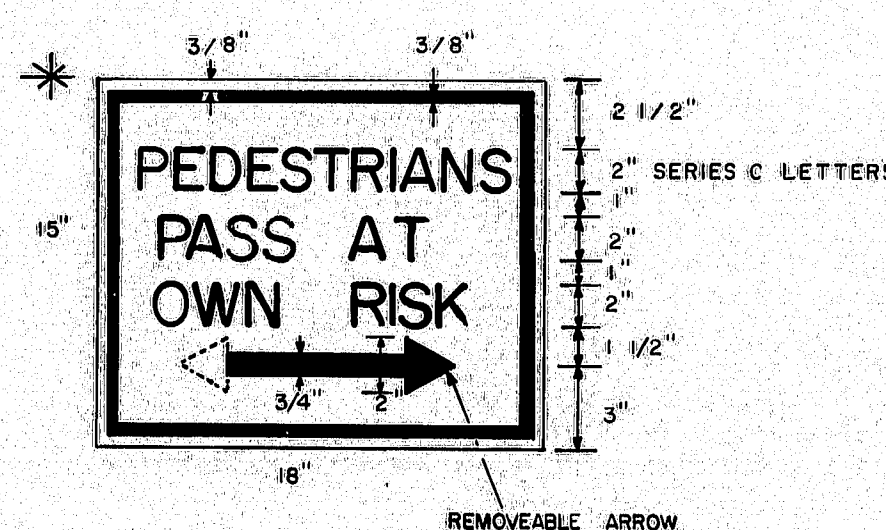
**SIDEWALK CLOSURE
WITHOUT ALTERNATE SIDEWALK**



* NON-REFLECTORIZED WHITE BACKGROUND, BLACK TEXT AND BORDER-2" SERIES C UPPER CASE LETTERS

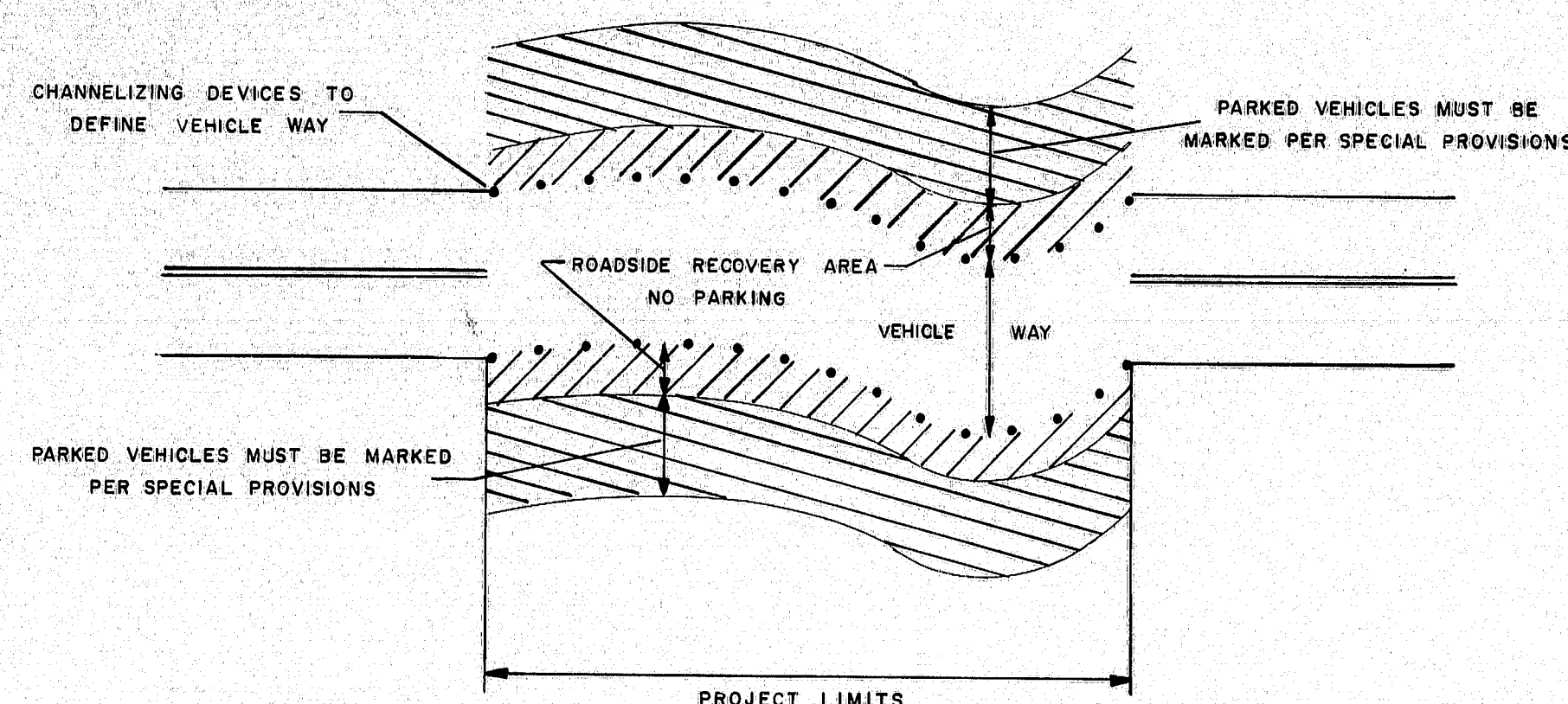


**SIDEWALK CLOSURE
WITH ALTERNATE SIDEWALK**



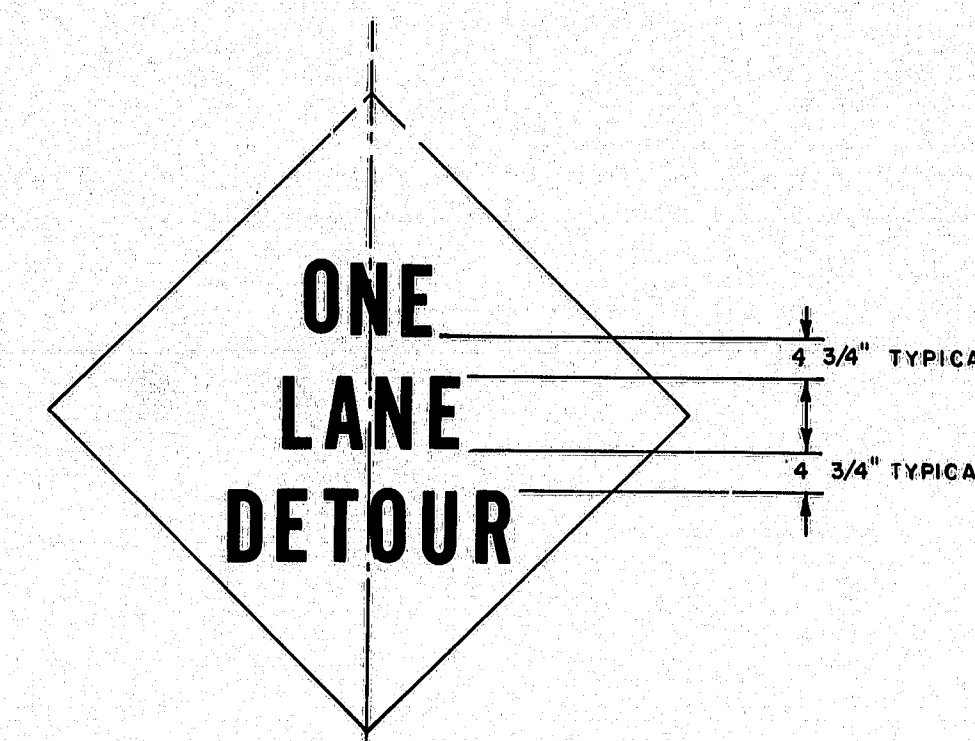
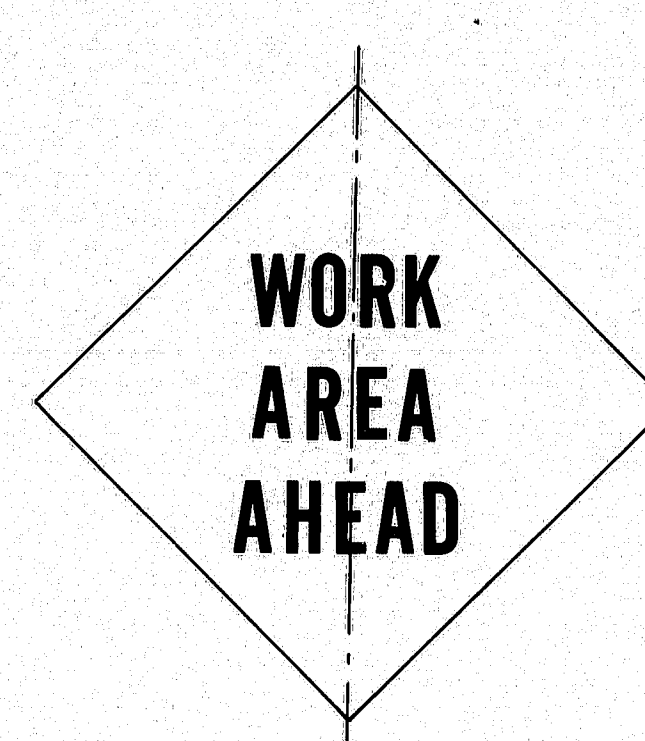
MEDIAN CROSSOVER

ALL DIMENSIONS AND OTHER REQUIREMENTS AS
SPECIFIED IN THE SPECIAL PROVISIONS



ROADSIDE RECOVERY AREA

CONSTRUCTION WARNING SIGN DETAIL



1. Letter size shall be 8" Series 'D'.
2. Border dimensions and legend design shall conform to "Standard Highway Signs".

GENERAL NOTES

1. Distances shown for sign placement are nominal, exact locations shall be determined by the Engineer.
2. Grades on temporary roadways through the construction zone used by the public shall not exceed 10 percent.
3. Advisory speed consistent with prevailing conditions shall be as determined by the Engineer.
4. Use amended signs when specified in the Special Provisions.
5. The length of tapers shall be determined from the following formulae:

If S is equal to or less than 40 MPH
 $L = (W \times S \times S) / 60$

If S is equal to or greater than 45 MPH
 $L = WS$

Where:

L = taper length in feet
S = operating speed in MPH
W = width of roadway to be closed in feet

Taper lengths shall be rounded to the nearest five feet.

It may be required to extend lane closure tapers to provide a smooth transition where geometric alignment reduces sight distance.

6. The maximum longitudinal spacing of channelizing devices shall conform to the following:
 - (a) 50 feet through work areas
 - (b) A distance in tapers equal to the numerical value of the operating speed, i.e., 50 MPH = 45 feet
 - (c) In all areas not covered above maximum spacing shall be as follows:

Radius of curve	Spacing
50' to 300'	25'
300' to 700'	50'
700' to 1000'	75'
over 1000'	5 times the operating speed

The maximum transverse spacing in tapers shall be determined from the following formula:

$$D = (W \times S) / L$$

Where:

D = transverse spacing in feet
W = width of roadway to be closed in feet
L = taper length in feet
S = operating speed in MPH

7. B. DIMENSIONS AND LAYOUTS SHALL CONFORM TO THE STANDARD HIGHWAY SIGN BOOKLET.

PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAILED	
CHECKED	
REVISIONS	
FIELD CHANGES	

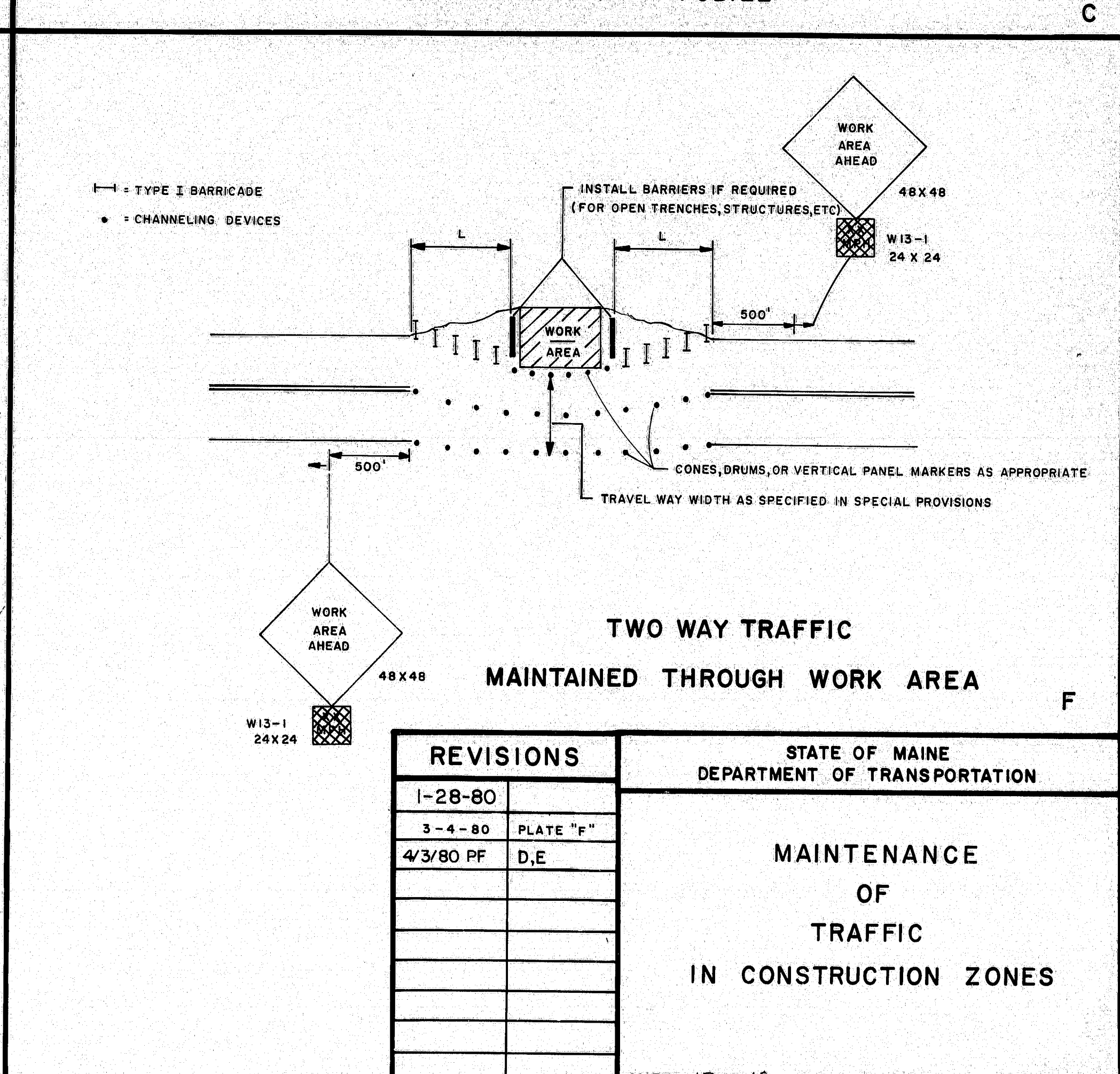
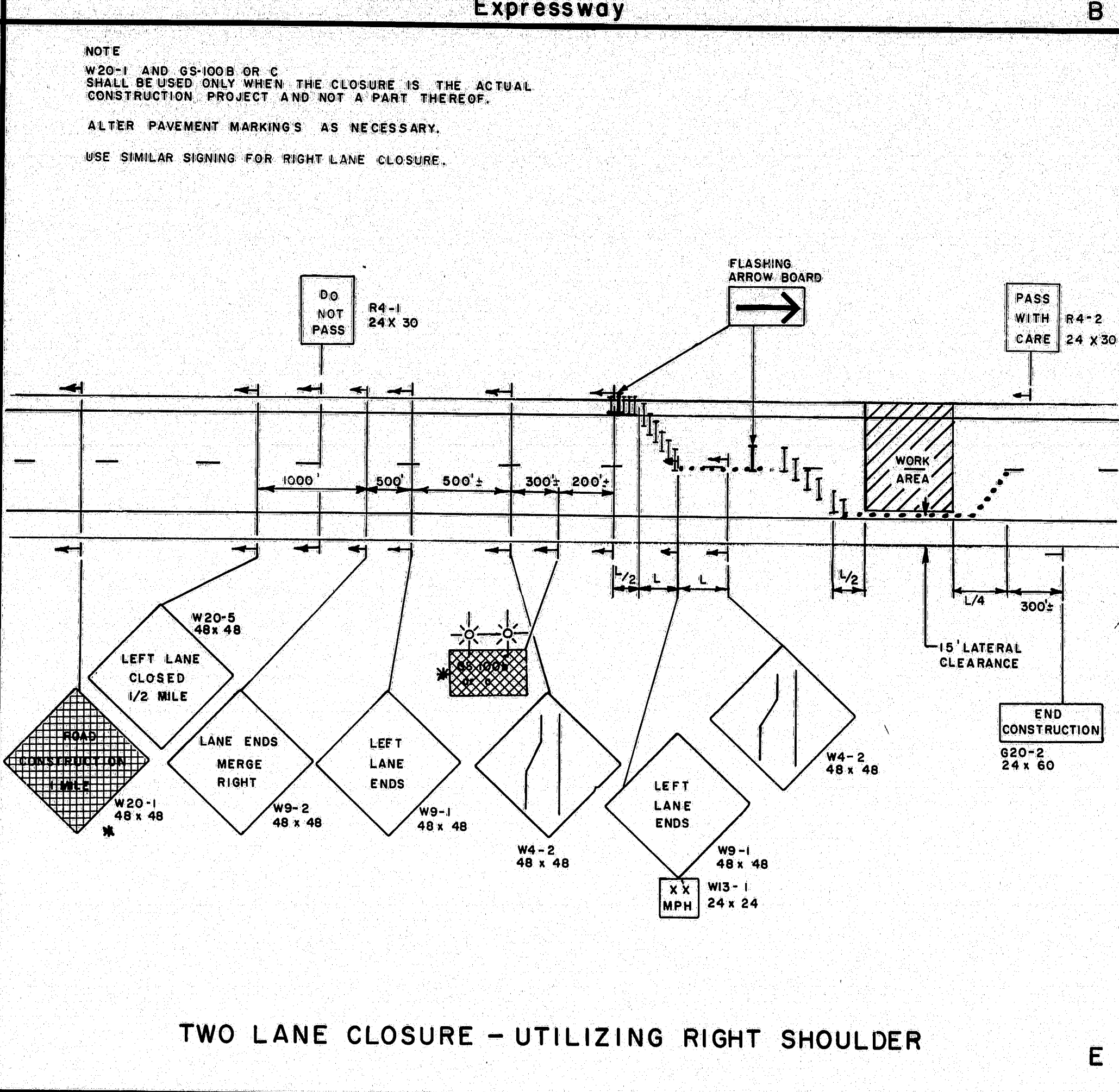
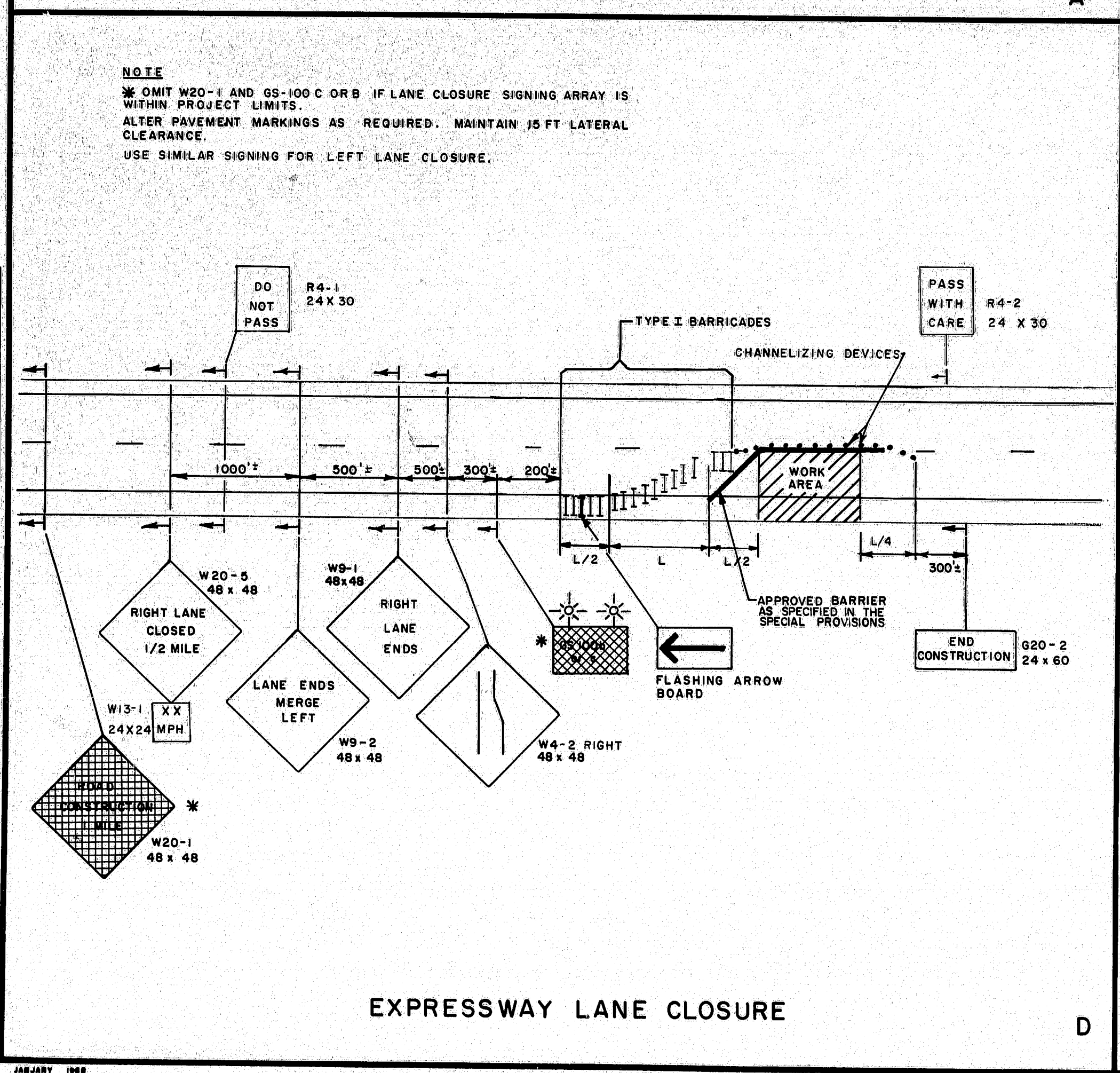
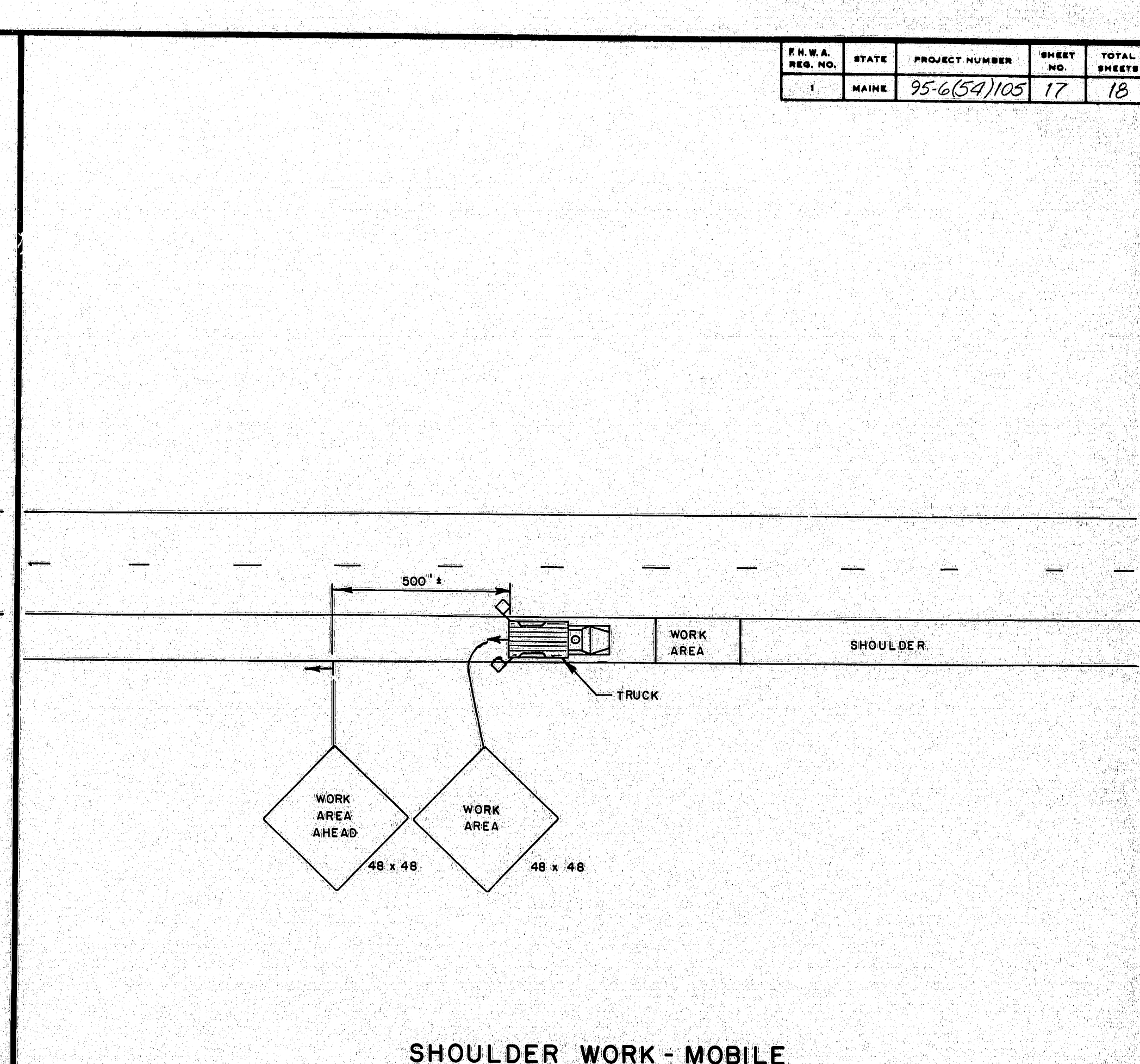
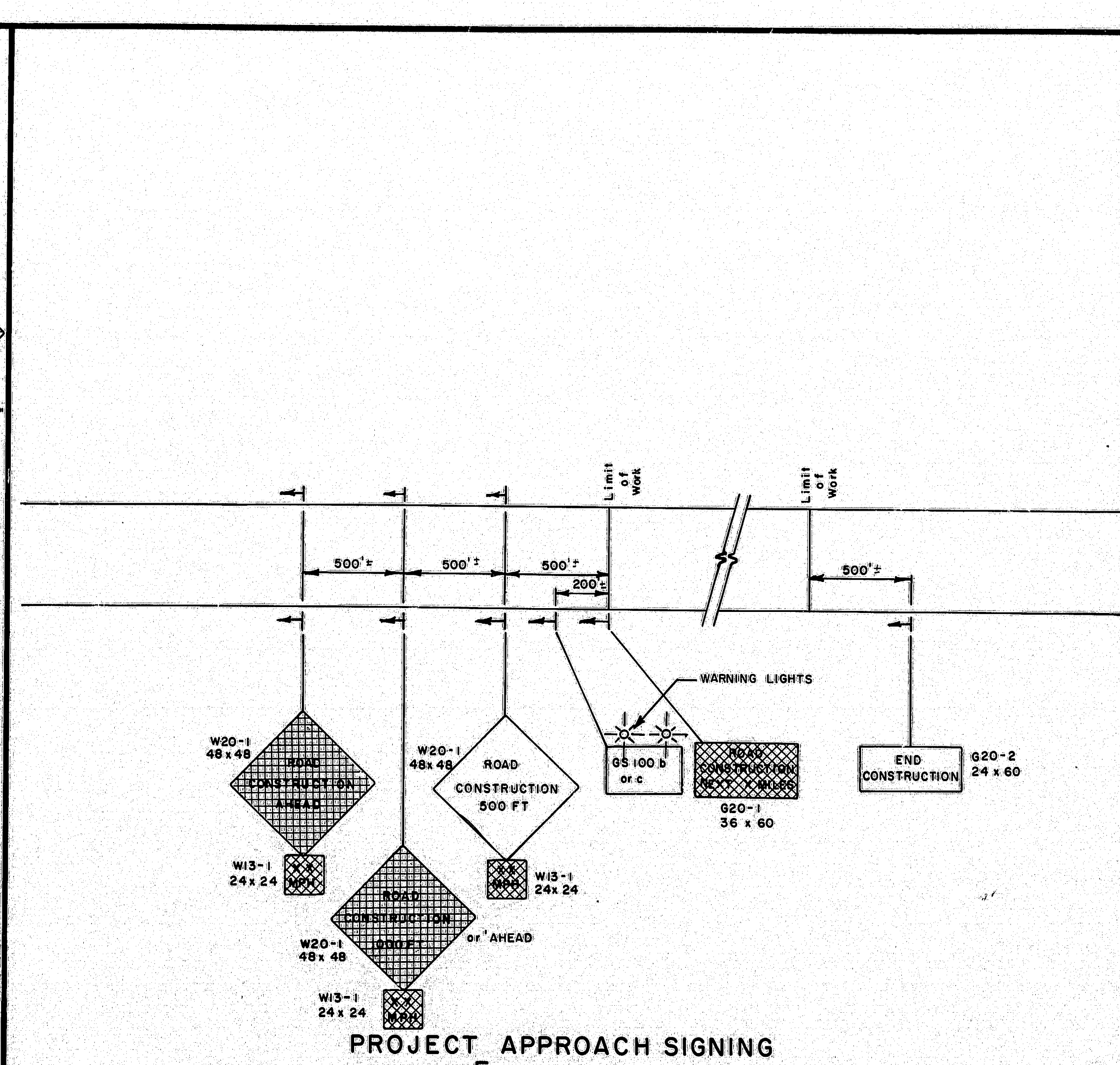
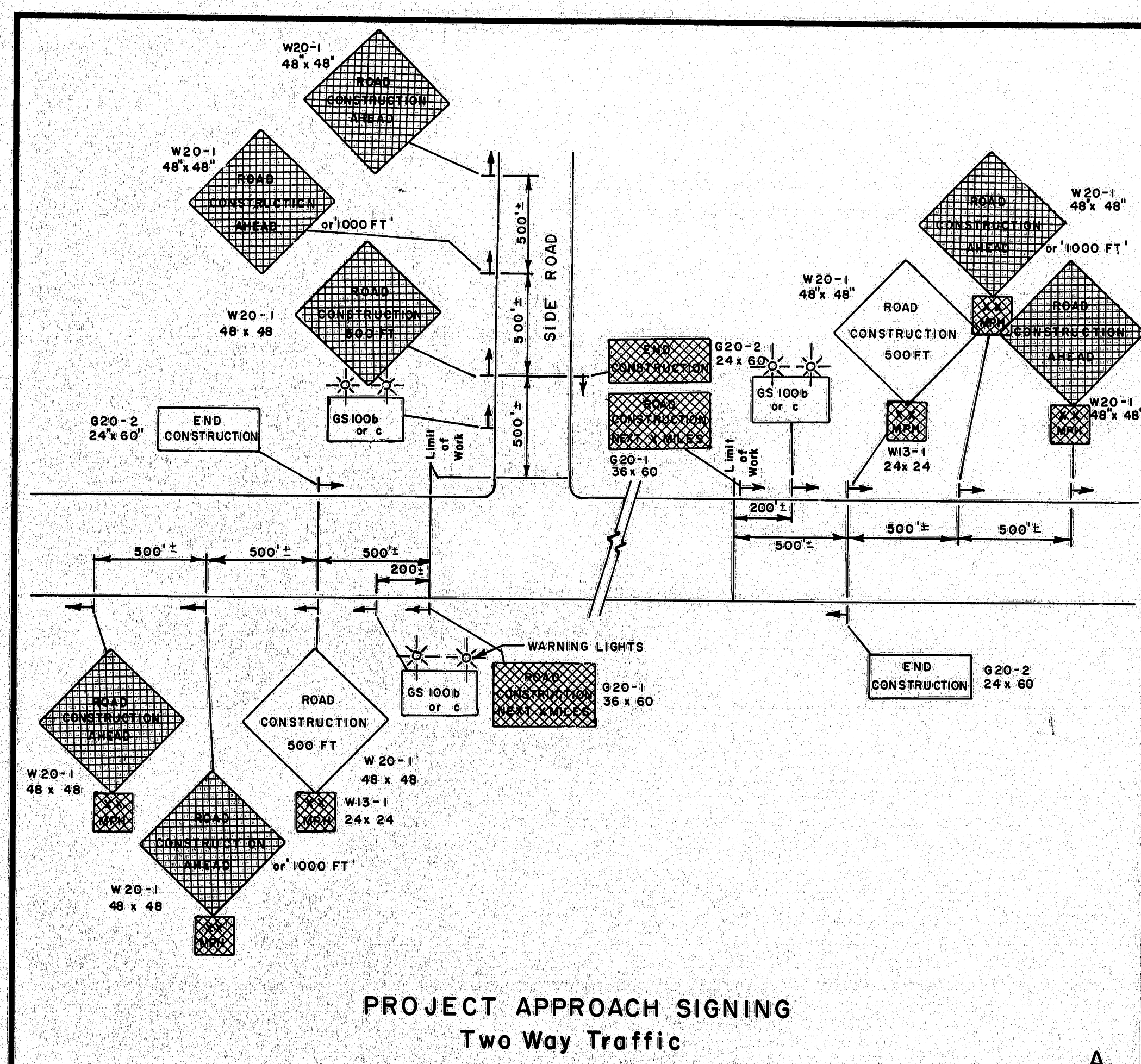
REVISIONS		STATE OF MAINE DEPARTMENT OF TRANSPORTATION	
3-4-80	GENERAL NOTES		
4/3/80 PF	A,B,C,GH		

**MAINTENANCE
OF
TRAFFIC
IN CONSTRUCTION ZONES**

SHEET 16 OF 18 AUGUSTA, MAINE

172-130

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	17	18



PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAILED	
CHECKED	
REVISIONS	
FIELD CHANGES	

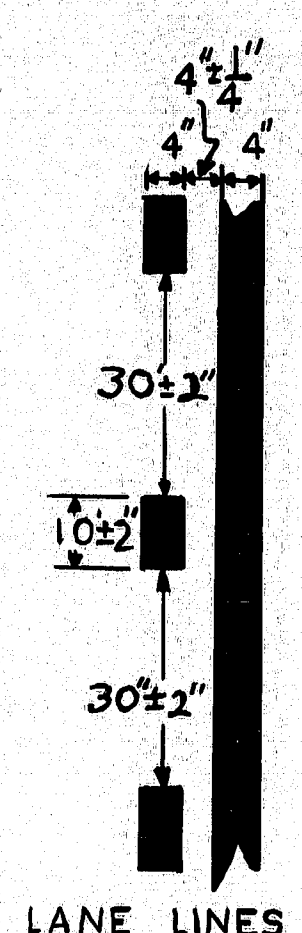
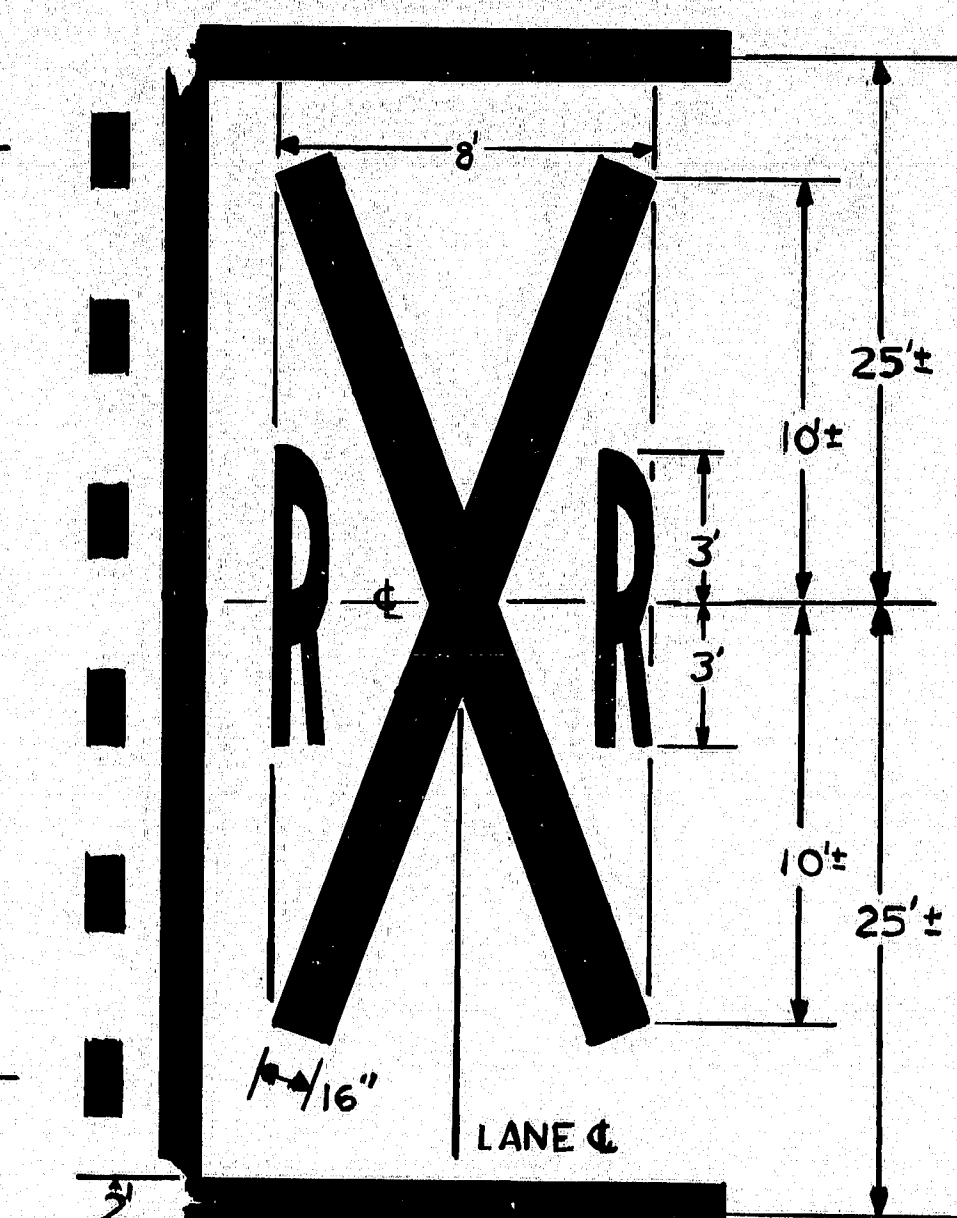
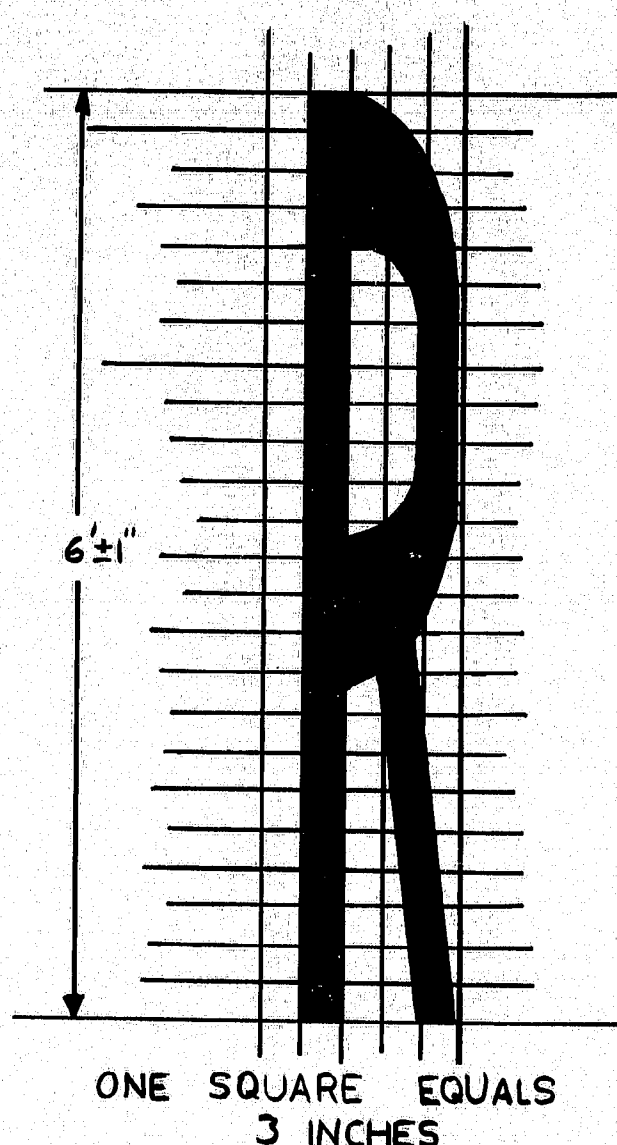
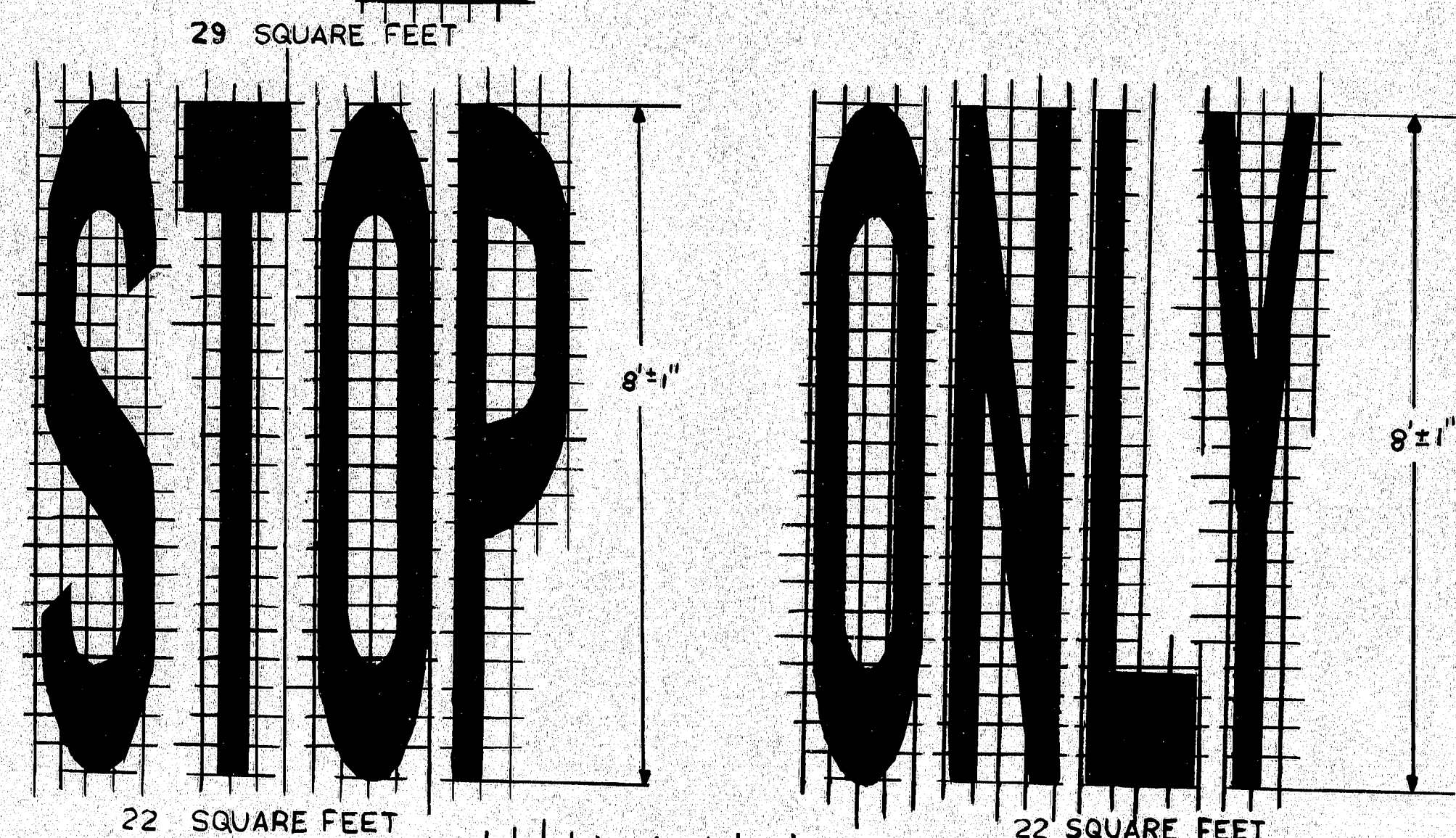
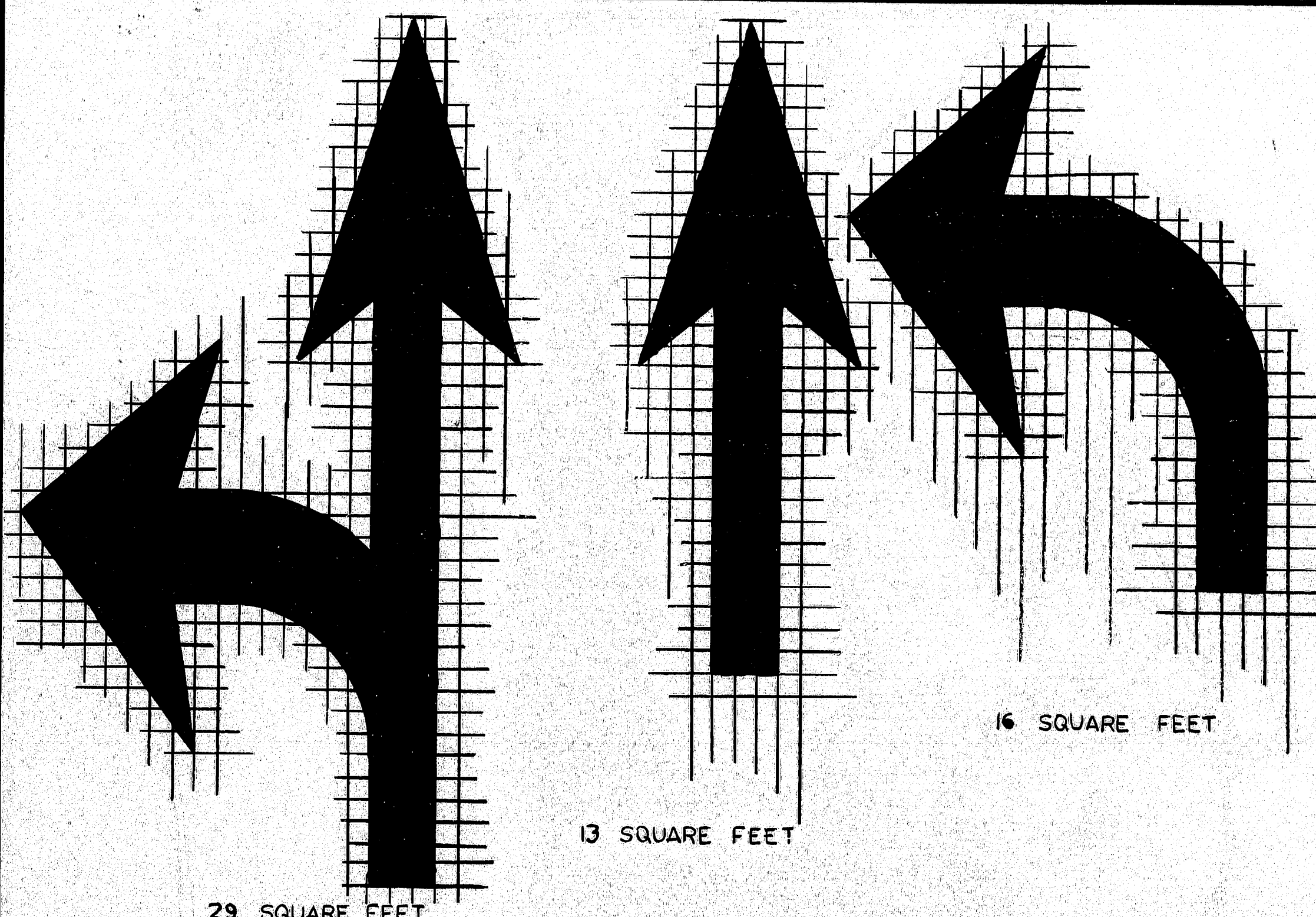
REVISIONS
1-28-80
3-4-80 PLATE "F"
4/3/80 PF D,E

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC IN CONSTRUCTION ZONES

SHEET 17 OF 18 AUGUSTA, MAINE JULY, 1979

172-131

F.H.W.A. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	95-6(54)105	18	18



STROKE WIDTH AND LINE WIDTH VARIANCE SHALL BE $\pm 1/4$ INCH FROM DIMENSIONS SHOWN.

DIMENSIONS SHOWN ARE PAY DIMENSIONS

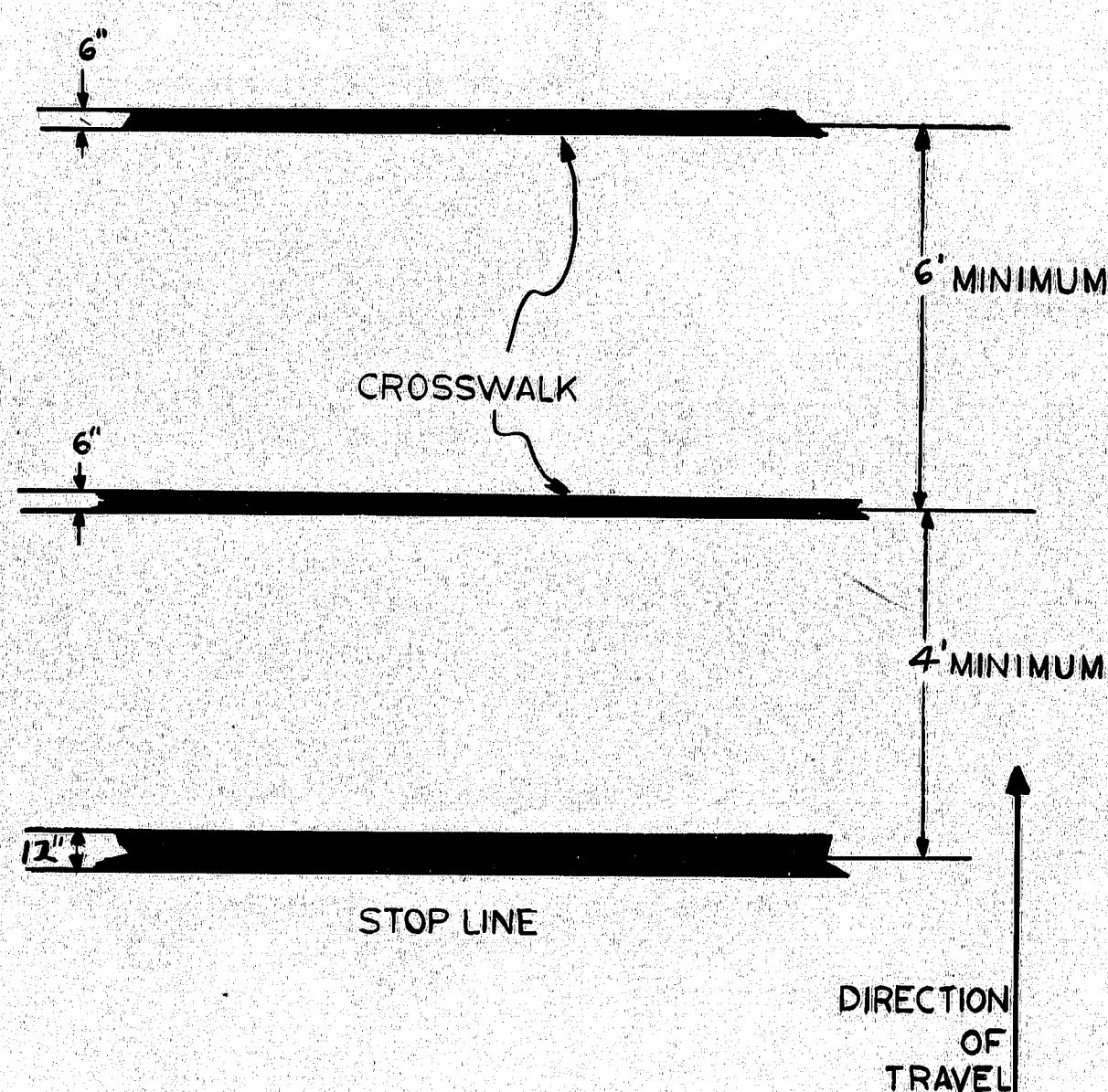
GRID IS MARKED IN FOUR INCH INTERVALS EXCEPT AS NOTED

SYMBOLS AND LETTERS SHALL BE PROPORTIONED ACCORDING TO GRID AS SHOWN.

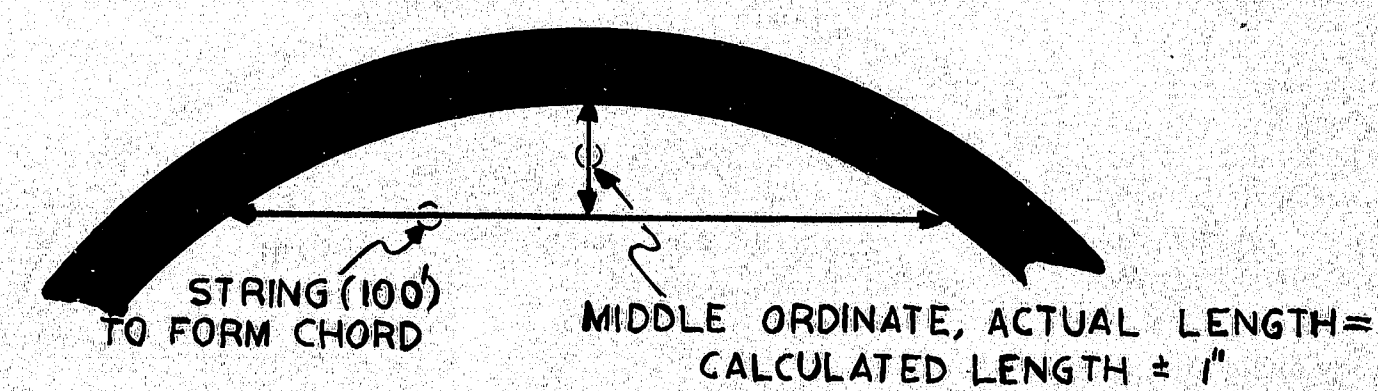
THE SPACE BETWEEN CHARACTERS SHALL BE ONE UNIT, BUT VISUAL SPACING MAY BE USED.

SPACING BETWEEN SYMBOL AND STOP LINE SHALL BE A MINIMUM OF 20 FT OR AS DIRECTED BY ENGINEER.

SPACING BETWEEN SYMBOL AND SYMBOL SHALL BE A MINIMUM OF 50 FT OR AS DIRECTED BY ENGINEER.



TOLERANCE FOR PAVEMENT MARKING LINES



STRING (100') PARALLEL TO PAVEMENT MARKING LINE

DISTANCE FROM THE STRING TO THE EDGE OF THE PAVEMENT MARKING LINE SHALL NOT BE MORE THAN 1'

PROJECT DESIGN ENGINEER	DATE
BY	
DESIGN - DETAILED	
CHECKED	
REVISIONS	
FIELD CHANGES	

REVISIONS	
DEC 79	

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

STANDARD PAVEMENT
MARKINGS

(PART 3 1978 MUTCD)

SHEET 18 OF 18 AUGUSTA, MAINE

172-132